

(DRAFT) AGENDA
Regular Meeting – Bremerton Planning Commission
(Subject to PC approval)
May 19, 2025
5:30 P.M.
345 6th Street, Bremerton
First Floor Chambers

Zoom Meeting Option

<https://us02web.zoom.us/j/89651517986?pwd=b0dnVVhvSmd5KzFGL0ljS1NwVjJ3dz09>

Webinar ID: 860 8962 6977

Password: 955016

Dial by your location:

+1 253 215 8782 US (Tacoma)

+1 669 900 6833 US (San Jose)

Remote Participation:

To provide testimony on items listed on the agenda “Raise Your Hand” on Zoom Press *9 on your phone

One tap mobile:

+1-253-215-8782 86089626977# *955016#
US (Tacoma)

If the public would like to address the Commission on any item that is not on the agenda it must be done in person, and not via Zoom, unless prior arrangements were made through the Project Assistant by calling (360) 473-5269 at least 24-hours prior to the meeting.

I. CALL TO ORDER

II. ROLL CALL

III. APPROVAL OF THE AGENDA

IV. APPROVAL OF MINUTES: April 21, 2025

V. PUBLIC MEETING

A. Call to the Public: In-person public comments on any item not on tonight’s agenda.

B. Public Hearing:

1. Development Regulations, 2024 Comprehensive Plan

VI. BUSINESS MEETING

A. Chair Report: Nick Wofford

B. Director’s Report: Andrea Spencer

C. Old Business:

D. New Business: Zoning Code Amendments, Upcoming Workshops

VII. ADJOURNMENT: The next regular meeting of the Planning Commission will be held on June 16, 2025.

DRAFT

CITY OF BREMERTON

PLANNING COMMISSION MINUTES OF REGULAR MEETING April 21, 2025

CALL TO ORDER:

Chair Wofford called the special meeting of the Bremerton Planning Commission to order at 5:30 p.m.

Chair Wofford introduced new Planning Commissioner Ana Doehring.

ROLL CALL

Commissioners Present

Chair Wofford
Vice Chair Tift
Commissioner Browning
Commissioner Doehring
Commissioner Miller
Commissioner Paauw
Commissioner Pedersen

Staff Present

Andrea Spencer, Director, Department of Community Development
Garrett Jackson, Planning Manager, Department of Community Development
Vicki Grover, Engineering Project Manager, Public Works and Utilities
Janelle Siefert, Planner, Department of Community Development
Sharon Schwartz, Project Assistant, Department of Community Development

Quorum Confirmed

CHAIR CALL FOR MODIFICATIONS TO AGENDA

The agenda was accepted as presented.

APPROVAL OF MINUTES

VICE CHAIR TIFT MOVED TO APPROVE THE PLANNING COMMISSION MEETING MINUTES OF FEBRUARY 24, 2025, AS PRESENTED; COMMISSIONER PEDERSEN SECONDED THE MOTION, WHICH CARRIED WITH SIX (6) AYES (BROWNING, MILLER, PAAUW, PEDERSEN, TIFT AND WOFFORD) AND ONE (1) ABSTENTION (DOEHRING).

PUBLIC MEETING

Call to the Public (public comments on any item not on the agenda)

No public comments were received.

PUBLIC HEARING:

Hearing: 2024 Comprehensive Plan Update – A Power Point presentation was provided by Community Development Planning Manager, Garrett Jackson.

During Mr. Jackson's presentation he provided information related to the 2024 Comprehensive Plan, including the Draft Comprehensive Plan and associated appendices.

Chair Wofford opened the Public Workshop to receive testimony from the public. Comments received from Mark Goldberg, Brandon Bishop, Jamie Clough and Marla Powers.

Mark Goldberg – Noted being in multifamily development for over 45 years, and the need for land to be properly zoned. Believes having no parking downtown will not work and will be a disaster. Mentioned Lions Park 10 units per acre with \$3 million assessed values of properties will mean someone will have to pay \$300,000 to put one dwelling unit. Says he had this conversation with Andrea Spencer years ago when they rezoned with the huge apartment complexes on the bluff and have 40 units per acre.

Brandon Bishop – Noted working for Knapton Development and expressed thanks to Garrett Jackson and Andrea Spencer for having them tonight, and voiced support for the vast majority of the new plan. Would like to partner and work with the City to build additional housing. Is pro-Navy and want to provide housing for those workers. Is pro 8 story limit on housing in the Downtown core and more units per land.

Jamie Clough – On behalf of Kitsap Building Association, supports the City of Bremerton’s 2024 comprehensive Plan update which takes steps for future population growth, expanding home options and modernizing land use policies. Expressed appreciation for the City’s efforts to plan for infrastructure housing and economic growth in a balanced way. Looks forward to continuing partnership to ensure Bremerton remains a place where people of all incomes can find a home.

Marla Powers – Works as an environmental planner for the Port Gamble S’Klallam Tribe and expressed thanks to Garrett Jackson for his specific and direct outreach to the tribe for comments on the EIS and Comprehensive Plan. The tribe appreciates his interest and concerns for their comments and feedback.

Questions and comments were offered by Planning Commissioner Tift, with responses provided by Garrett Jackson.

COMMISSIONER PEDERSON MOVED AND COMMISSIONER MILLER SECONDED TO RECOMMEND THAT THE CITY COUNCIL ADOPT THE COMPREHENSIVE PLAN UPDATE AS SHOWN IN ATTACHMENT A OF THIS REPORT BASED ON THE FINDINGS AND CONCLUSIONS PRESENTED IN ATTACHMENT B. MOTION CARRIED WITH SIX (6) AYES (BROWNING, MILLER, PAAUW, PEDERSEN, TIFT AND WOFFORD) AND ONE (1) ABSTENTION (DOEHRING).

Hearing: Development Regulations, 2024 Comprehensive Plan – A Power Point presentation was provided by Community Development Planning Manager Garrett Jackson.

During Mr. Jackson’s presentation he provided information related to the Development Regulations proposed to be implemented in the 2024 Comprehensive Plan, which includes Draft regulations for Bremerton Municipal Code Title 20 and several subarea plans.

Chair Wofford opened the Public Workshop to receive testimony from the public. No public comments were received.

Questions and comments were offered by Planning Commissioners Miller, Tift and Paauw, with responses provided by Garrett Jackson and Andrea Spencer.

VICE CHAIR TIFT MOVED AND COMMISSIONER BROWNING SECONDED TO RECOMMEND THAT THE CITY COUNCIL ADOPT DEVELOPMENT REGULATIONS AS SHOWN IN ATTACHMENT A OF THIS REPORT BASED ON THE FINDINGS AND CONCLUSIONS PRESENTED IN ATTACHMENT B. MOTION CARRIED WITH SIX (6) AYES (BROWNING, MILLER, PAAUW, PEDERSEN, TIFT AND WOFFORD) AND ONE ABSTENTION (DOEHRING).

BUSINESS MEETING

Chair Report

DRAFT

Chair Wofford

No Report.

Director Report

Andrea Spencer highlighted that we have started to provide an After Meeting Action Summary page to the front of Planning Commission meeting packets for those who may access meeting materials at later dates to better understand the directions and outcomes of these meetings. Thanked and Congratulated Garrett Jackson on his hard work over the last several years while working on the Comprehensive Plan updates.

Old Business

None.

New Business

Andrea Spencer and Garrett Jackson informed the Planning Commission that they will continue to review potential Zoning Code Amendments in 2025 and provided a list of workshop items to be reviewed and considered. Feedback provided by Planning Commissioners Wofford, Paauw, Tift, Miller and Pedersen.

ADJOURNMENT

The meeting was adjourned at 6:57 p.m.

Respectively Submitted by:

Andrea L Spencer, AICP
Executive Secretary

Nick Wofford, Chair
Planning Commission

**CITY OF BREMERTON, WASHINGTON
PLANNING COMMISSION AGENDA ITEM**

AGENDA TITLE: Public Hearing: 2024 Comprehensive Plan Update

PRESENTED BY: Garrett Jackson, Planning Manager; (360) 473.5289

EXECUTIVE SUMMARY

The following report provides information related to Development Regulations proposed to implement the 2024 Comprehensive Plan for the Planning Commission to consider at the public hearing, this includes the Draft regulations for the Bremerton Municipal Code Title 20 and several subarea plans.

PROJECT OVERVIEW

“Bremerton2044” is the title of the City of Bremerton’s Comprehensive Plan Update, which focuses on evaluating which aspects of the plan are working and what needs to be adjusted. The update is required by the Washington State Growth Management Act (GMA) in order to demonstrate that Bremerton has the capacity to accommodate population and employment increases forecast to the year 2044. Population estimates indicate that the City will grow to 63,757 persons and 58,258 jobs by 2044. As the US Census Bureau estimates that Bremerton’s population was 43,505 with 44,083 jobs in 2020, this means the City is forecast to increase its population by over 46%, with a 32% increase in the number of jobs. The Comprehensive Plan must also be consistent with regional planning goals established in Puget Sound Regional Council’s (PRSC) [Vision 2050](#) and County level planning efforts per the Kitsap Regional Coordinating Council (KRCC) [Countywide Planning Policies](#). The overarching principles and general concepts within the 2016 Comprehensive Plan continue to be applicable, however, some alterations are necessary to reflect changes in State and regional planning standards and to ensure that the document still represents the community.

ATTACHMENTS

Attachment A: 2024 Comprehensive Plan

Attachment A.1: Title 20 Zoning Code Amendments

Attachment A.2: Puget Sound Industrial Center Subarea Plan

Attachment A.3: Harrison Heights Subarea Plan

Attachment A.4: Bay Vista Subarea Plan

Attachment A.5: East Park Subarea Plan

Attachment B: Draft Planning Commission Findings & Conclusions

STAFF RECOMMENDATION

Staff recommends that the Planning Commission conduct a public hearing on the proposed Development Regulations, consider public testimony, deliberate, and forward a recommendation to the City Council for adoption.

MOTION FOR CONSIDERATION

Motion: Move to recommend that the City Council adopt Development Regulations as shown in Attachment A of this report based on the Findings and Conclusions presented in Attachment B.

INTRODUCTION

As discussed at the [April 21, 2025 Planning Commission meeting](#), Title 20 of the Bremerton Municipal Code (BMC), and individual subarea plans, regulate Land Use for the City, which includes the uses, development regulations, and permitting procedures related to development of individual lots. This month's hearing furthers Zoning Code Amendments intended to support implementation of the 2024 Comprehensive Plan Update. Proposed amendments were published concurrently on the City webpage April 7th with the 2024 Comprehensive Plan Update Final Environmental Impact Statement (FEIS). Notification of the FEIS, and availability of Final Draft Documents for the Comprehensive Plan, was sent at that time to agencies, affected tribes, and other interested parties.

DEVELOPMENT REGULATIONS

A summary of proposed development regulation updates is provided below.

1. [Bremerton Municipal Code \(BMC\) Title 20 \(Attachment A.1\)](#). Proposed alterations are provided in legislative markup, where new language is underlined and language proposed to be deleted is in ~~cross through~~. To manage the overall length of the attachment, only sections where revisions are proposed are included; links to the original language are provided to reference existing code.
 - [BMC 20.12 Boundary Line Adjustment \(BLA\)](#): The BLA process is intended to permit the adjustment of property lines for existing lots, including their aggregation in some cases. Amendments are intended for clarification of the code process, and to remove unnecessary barriers to execute BLAs.
 - [BMC 20.46 Family Day Care Provider](#): Consistent with [RCW 36.70A.450](#), the proposed code provides criteria for family day care for up to 12 children. The use is permitted in any residential or mixed-use zone; which is all zones permitting residential uses in the City. Requirements per this section follow state guidance listed in statute.
 - [BMC 20.46 Co-Living Housing](#): Consistent with [HB 1998](#) the proposed code provides criteria for co-living housing, which is a residential development with sleeping units that are independently rented and provide living and sleeping space, in which residents share kitchen facilities with residents of other units in the building. Requirements per this section follow state guidance listed in statute.
 - [BMC 20.46 Neighborhood-Scale Commercial](#): Citizens, Planning Commissioners, and members of the City Council have expressed interest in permitting small-scale commercial uses in residential zones. The intent is to enhance neighborhoods and promote walking and bicycle modes of transportation. The proposal would add small-scale restaurants, drinking places, and groceries with a Type II Conditional Use Permit in all residential zones.
 - [BMC 20.54.120 New Housing In Nonconforming Buildings](#): Consistent with [RCW 35.21.990](#), the proposed code provides criteria for new housing in nonconforming buildings. This code section permits the conversion of existing commercial buildings that have received an occupancy permit at least 3-years prior to permit submittal. The code criteria establishes a State required 50% density bonus increase for applicable development beyond that listed in the underlying zone.
 - [BMC 20.58.060 Residential Cluster Development \(RCD\)](#): Proposed alterations to the RCD section of the code were initiated to remove listed minimum parking

requirements. Additional provisions are proposed to account for the Planning Commission's stated preference for a 15% open space requirement, in order to mirror requirements in Kitsap County. Lot requirements are also proposed for removal, as they related to the development of single-family homes, and additional uses are now available in underlying zoning districts negating the need for this criteria.

- **BMC 20.70 Market-based Parking Standards:** In an effort to create more affordable housing, and also promote development patterns that encourage walking, biking, and transit, the Planning Commission has directed Staff to transition existing minimum parking standards to a market-based approach. This means that the private sector will determine how many parking spaces are required for development projects, rather than the City of Bremerton imposing a regulatory requirement. It is well documented that parking represents very high costs when developing projects, is often underused when constructed, and can be detrimental to other community needs; please see the linked [Sightline Report](#) for additional information.

While the majority of parking standards are found in BMC 20.48 Off-Street Parking Requirements, there are several other sections with minimum parking requirements, including Subarea Plans. Proposed bicycle parking criteria are based on regulations previously approved by the Planning Commission with earlier versions of the Downtown Subarea Plan. Related code sections are as follows:

BMC 20.46.010 Accessory Dwelling Units, BMC 20.46.030 Home Occupations, BMC 20.46.070 Adaptive Reuse of Commercial Buildings, BMC 20.46.090 Temporary Encampment Permit, BMC 20.46.170 Cottage Housing, BMC 20.46.180 Group Residential Class I, BMC 20.46.190 Senior Housing Complex, BMC 20.46.200 Manufactured Home Park, BMC 20.48 Off-street Parking Requirements, BMC 20.54.060 Nonconforming Uses, BMC 20.54.080 Nonconforming Landscaping, BMC 20.58.060 Residential Cluster Development, several conditional uses in the Low Density Residential zone per BMC 20.60.040, BMC 20.70 District Center Core, several conditional uses in the Medium Density Residential zone per BMC 20.78.040, BMC 20.79 High Density Residential, BMC 20.82 Neighborhood Business, BMC 20.96 City Utility Land, and BMC 20.97 Watershed.

The Planning Commission has held several workshop discussions related to the removal of minimum parking standards, and at the April meeting further direction was provided to Staff. It is noteworthy that Senate Bill [SB 5184](#) passed the Washington State Legislature, with the Governor anticipated to sign the legislation into law. Among other provisions, this legislation limits a City to requiring 1 parking space per single-family residence, .5 parking spaces per multifamily residence, and 2 parking spaces per 1,000 square feet of nonresidential uses. Several parking exemptions are listed in the legislation (affordable housing, residences under 1,200 square feet, etc.) and those have been incorporated into Attachment A.1. Other State mandates related to gravel and grass paver parking per [SB 6015](#) are also incorporated into the proposed amendments.

The Planning Commission requested an option be presented in the code, where only single-family homes located in the Low Density Residential zone would be required to provide minimum parking. This code section is provided as BMC 20.60.060(i)(1),

where single-family homes would be required to provide one parking space per unit, consistent with SB 5184.

2. [*Puget Sound Industrial Center \(Attachment A.2\)*](#). Minor updates are proposed to the Puget Sound Industrial Center Subarea Plan consistent implementing market-based parking standards. Specific legislative mark up is provided later in this report, while the finished revised pages are provided as Attachment A.2.
3. [*Harrison Heights Subarea Plan \(Attachment A.3\)*](#). Minor updates are proposed to the Harrison Heights Subarea Plan consistent implementing market-based parking standards. Specific legislative mark up is provided later in this report, while the finished revised pages are provided as Attachment A.3.
4. [*Bay Vista Subarea Plan \(Attachment A.4\)*](#). Minor updates are proposed to the Bay Vista Subarea Plan consistent implementing market-based parking standards. Specific legislative mark up is provided later in this report, while the finished revised pages are provided as Attachment A.4.
5. [*East Park Subarea Plan \(Attachment A.5\)*](#). Minor updates are proposed to the East Park Subarea Plan consistent implementing market-based parking standards. Specific legislative mark up is provided later in this report, while the finished revised pages are provided as Attachment A.5.

STAFF RECOMMENDATION

Staff recommends that the Planning Commission conduct a public hearing on the proposed Comprehensive Plan Update, consider public testimony, deliberate, and forward a recommendation to the City Council for adoption.

MOTION FOR CONSIDERATION

Motion: Move to recommend that the City Council adopt the Comprehensive Plan Update as shown in Attachment A of this report based on the Findings and Conclusions presented in Attachment B.

Puget Sound Industrial Center Subarea Plan: Amended Code Sections

Section 4; 4.4.040 (pg 52)

- c. Minimum Parking Requirements. Development is exempt from providing automobile off-street parking spaces, except as otherwise required for conformance with the Federal Americans with Disabilities Act (ADA) and the State of Washington. Minimum parking standards shall be in accordance with BMC Chapter 20.48 Off-Street Parking Requirements, except as specified in Paragraph (1) of this subsection as well as in Subsection (e) below.
 - 1. Parking reductions may be allowed, at the discretion of the Director, if a parking analysis is completed and indicates that sufficient parking is available to meet demand.

Section 4; 4.040d (pg 53)

d. Shared Parking Area Incentives Reductions

- 1. ~~The amount of To qualify for incentives listed per Section 5.040, Exhibit PSIC-33, off-street parking for required in Subsection (c) above may be reduced where multiple buildings/tenants may share a common, centrally located parking area. Reduction shall be based on the type and mix of uses, as follows, except that these reductions shall not be used in conjunction with reductions specified in Subsection (c) above:~~
 - ~~i. Properties with only industrial or support retail and service uses: 30% reduction~~
 - ~~ii. Properties with retail or other uses: 10% reduction~~
 - ~~iii. Mixed retail/office uses with at least a 4:1 ratio of retail to office: 20% reduction~~
 - ~~iv. 50% reduction for retail uses of less than 5,000 sq. ft. when they are developed as part of a business park that contains at least 60% industrial or support retail and service uses.~~

Section 4; 4.040h (pg 53)

h. Bicycle Facilities. Bicycle parking shall be provided at 10% of the required automobile spaces consistent with BMC 20.48. Please see BMC 20.48.180(b) for bicycle parking requirements and Section 5.3.170 for recommended Design Guidelines.

Section 5; 5.040, Exhibit PSIC-33 (pg 73)

Shared Parking	Shared parking is provided that serves two or more tenants. No additional parking outside of the shared lot(s) may be provided. Shared parking lots shall be located within a <u>1,200 foot</u> radius of the front door of the building. Number of parking stalls is no more than 50% greater than minimum requirement in Section 4.4.040(c).	5 points
----------------	---	----------

Harrison Heights Subarea Plan: Amended Code Sections

Section 4.2.010 (pg 41)

regulations.

LU1-Cen(F): Implement parking standards that prioritize Active Transportation, public transit, housing and community uses, rather than expansion of an automobile-based built environment.

~~LU1-Cen(F): Implement parking ratios that reflect the least amount of spaces required for development approval where transportation options other than the automobile are available to serve travel needs.~~

LU2-Cen(A): Pre-qualify key areas and sites for environmental permitting through such tools as subarea plans and related programmatic Environmental Impact Statement's. Work toward enabling development in Centers to proceed as a Planned Action under the State Environmental Protection Act (SEPA) including coordination with the local tribal government for protection of treaty cultural and natural resources.

LU2-Cen(B): Coordinate with Kitsap Transit to provide transit access to centers.

LU2-Cen(C): Provide incentives and flexibility that encourage and enable development in Centers, including alternative parking options like payment in lieu of parking spaces.

LU3-Cen(A): Provide recreation opportunities within centers including access to the shoreline.

LU4-Cen(A): Improve and provide for walkability, and other nonmotorized transportation routes throughout Centers and provide links between the centers and neighborhoods.

The Comprehensive Plan also includes a policy specific to the Eastside Employment Center (now called Harrison Heights)

LU2-EC(A): Provide flexibility in the setback, height, density, building footprint, and lot area development regulations to encourage redevelopment of this area and promote use of Low Impact Development (LID) techniques and Best Management Practices (BMPs).

Section 5.3.030 (pg 62)

5.3.030(E) Parking Requirements, pursuant to BMC 20.70.080, with the following exceptions:

1) ~~Minimum Required Spaces~~

~~(i) Senior Housing Complex .5 spaces per unit~~

~~(ii) Assisted Living .33 spaces per unit~~

~~(iii) Nonresidential 1 space per 1,000 gross square feet~~

~~(iv) Ground floor commercial space is exempt from off street parking requirements for the first 3,000 gross square feet~~

2) ~~Parking Reductions may be permitted per BMC 20.48.100.~~

Bay Vista Subarea Plan: Amended Code Sections

Zoning & Development Regulations (pg 21)

IV - ZONING & DEVELOPMENT REGULATIONS

A. LAND USE ZONES

This section of the Sub-Area Plan contains the zoning and development standards that will guide the redevelopment of Bay Vista and will implement the requirements of the site's zoning designation. The standards address the following elements:

Land uses/zones, including density, height, setbacks, building coverage and impervious surface coverage;

- Streetscapes that include amenities, (street trees, planting strips, benches, street lighting, etc.) and other design features that make streets interactive, walkable and enjoyable;
- A general landscape plan, including preservation of significant trees where possible;
- ~~Adequate off-street parking, which may modify standard requirements if a parking study demonstrates no impact on adjacent properties.~~

Table F-1 Parking Requirements (pg 32)

~~Table F-1 - Parking Requirements~~

Land Use Category	Automobile Spaces	Bicycle Spaces
	Minimum	
Residential:		
Single Family/Duplex	1.d.u.	N/A
Single Family ADU	1.d.u.	N/A
Multi-Family	1.d.u.	N/A
Apartment	1.d.u.	1:10d.u.
Business/Commercial:		
Retail	3:1,000 g.s.f.	1:5,000 g.s.f.
Offices	3:1,000 g.s.f.	1:2,500 g.s.f.
Restaurant	4:1,000 g.s.f. of seating area	1:500 g.s.f. of seating area
Medical/Dental	3.5:1,000 g.s.f.	1:2,500 g.s.f.
Hotel/Inn	1:room	N/A
All Other Commercial	3:1,000 g.s.f.	1:2,500 g.s.f.
Not Specified		
Educational	2:1,000 g.s.f.	1:1,000 g.s.f.
Community Facility	1:1,000 g.s.f.	1:1,000 g.s.f.

Bicycle Parking, and Other Required Parking Spaces (pg 33)

3. BICYCLE PARKING

- i. The required minimum number of bicycle parking spaces for each use category is shown on ~~Table F-1~~ shall be consistent with BMC 20.48. Bicycle parking spaces are meant to accommodate residents, employees, visitors, customers, messengers, and other persons.
- ii. Bicycle parking shall be located on site within 50 feet of well-used entrances and not farther from the entrance than the closest motor vehicle parking space. Bicycle parking may be located within city right-of-way in areas without building setbacks, subject to approval of the director and provided it meets the other bicycle parking requirements.

~~4. OTHER REQUIRED PARKING SPACES~~

- ~~i. Motorcycle Parking Requirements. Motorcycle spaces shall be required in non-residential areas only at a rate of 1 per 100 automobile spaces with a minimum of 1 motorcycle space provided.~~

East Park Subarea Plan: Amended Code Sections

<p>General Development Standards (pg 40)</p> <p>Accessory Dwelling Units (ADUs)</p> <ul style="list-style-type: none"> Entrance may be on any facade including that of the principal structure. No parking requirement for 1 bedroom or less One off street parking stall required for two or more bedrooms. Not included in maximum density requirements. Carriage units maximum garage structure 	<p>Medium Density (pg 46)</p> <p>lot requirements</p> <p>minimum lot size - 2000 sq ft minimum lot width - 34' minimum lot depth - 60' open space- 300 sq ft street setback- 5' minimum rear setback- 0' minimum at alley and 4' minimum to garage door side setback - 4' minimum parking - 2 stalls</p>	<p>High Density (pg 51)</p> <p>lot requirements</p> <p>minimum lot size - 1500 sq ft minimum lot width - 20' minimum lot depth - 78' street setback- 5' minimum side setback- 4' minimum or attached rear setback- 0' minimum to alley and 4' to garage door open space - 200 sq ft parking - 2 stalls</p>
<p>General Development Standards (pg 41)</p> <p>Parking</p> <p>Per East Park Development Standards</p> <ul style="list-style-type: none"> Loading space is not required for non-residential uses under 2500 sq ft. Maximum allowed garage structure limited to 1100 sq ft. 	<p>Medium Density (pg 47)</p> <p>lot requirements</p> <p>minimum lot size - 1700 sq ft minimum lot width - 34' minimum lot depth - 50' street setback - 5' minimum side setback- 4' minimum rear setback - 0' minimum at alley and 4' to garage door open space - 300 sq ft parking - 2 spaces</p>	<p>High Density (pg 52)</p> <p>lot requirements</p> <p>minimum lot size - 600 sq ft minimum lot width - 14' minimum lot depth - 44' street setback - 5' minimum side setback- 4' minimum or attached rear setback- 0' minimum to alley and 4' to garage door open space - 60 sq ft deck or porch parking - 1 stall per unit with 2 stalls for 2 or more bedroom units</p>
<p>Low Density (pg 43)</p> <p>lot requirements</p> <p>minimum lot size - 3600 sq ft minimum lot width - 50' minimum lot depth - 60' street setback - 5' minimum side setback - 4' minimum rear setback - 10' minimum open space - 400 sq ft parking - 2 stalls</p>	<p>Medium Density (pg 48)</p> <p>lot requirements</p> <p>minimum lot size - 1700 sq ft minimum lot width - 34' minimum lot depth - 50' street setback - 5' minimum side setback- 4' minimum rear setback- 4' minimum open space - 200 sq ft on lot and or common green parking - one off street space no more than 150' from the dwelling it serves</p>	<p>Mixed Use Density (pg 54)</p> <p>lot requirements</p> <p>street setback - 0' minimum other setback- 10' minimum residential parking - 1 per one bedroom unit 2 per 2 or more bedroom unit open space- 10% of site usable general retail and office and business services parking- 2500 sq ft exempt from parking and off street loading spaces</p>
<p>Low Density (pg 44)</p> <p>lot requirements</p> <p>minimum lot size - 3600 sq ft minimum lot width - 50' minimum lot depth - 60' street setback - 5' minimum side setback - 4' minimum rear setback- 10' minimum open space - 400 sq ft parking - 2 stalls</p>	<p>High Density (pg 50)</p> <p>lot requirements</p> <ul style="list-style-type: none"> residential lot minimum lot size - 2,500 sq ft minimum lot width - 46' minimum lot depth - 56' street setback - 5' minimum side setback- 4' minimum rear setback- 0' minimum to alley and 4' to garage door parking - 2 stalls open space - 300 sq ft carriage lot: minimum lot size - 850 sq ft minimum lot width - 32' minimum lot depth - 28' street setback - 5' min side setback - 4' minimum or 0' if attached rear setback - 0' minimum to alley and 4' to garage door parking- 1 stall open space - 0 sq ft 	

ATTACHMENT A.1

CHAPTER 20.12 LAND DIVISION

20.12.175 BOUNDARY LINE ADJUSTMENTS.

(a) Purpose. The purpose of this section is to provide ~~procedures and criteria for the review and approval of adjustments to boundary lines of existing lots of record which do legal lots or tracts in order to rectify defects in legal descriptions, to allow the enlargement of lots to improve or qualify as a building site, to achieve increased setbacks from property lines or sensitive areas, to correct situations wherein an established use is located across a lot line, or for other similar purposes.~~ A boundary line adjustment shall not be used to ~~subdivide land, i.e., it shall not create any new or additional lot, tract or parcel, consistent with the provisions of RCW 58.17.040, RCW 58.04.007 and WAC 458-61A~~ The boundary line adjustment is not for the purpose of avoiding public improvement requirements that would be associated with a replat or other new land division approval.

(b) Procedure.

(1) A request for a boundary line adjustment is processed as a Type I Director's decision pursuant to Chapter [20.02](#) BMC.

~~(2) Boundary line adjustments may also be accomplished as part of a plat or short plat.~~

~~(3) At the Director's discretion, a site plan review or separate permit may be required prior to the submittal of a BLA or concurrently with the BLA permit.~~

(c) Application Submittal Requirements. An applicant shall submit a complete boundary line adjustment application to the City. The following shall constitute accompany a complete application for the purposes of this chapter:

(1) A Boundary Line Adjustment Application form. The signatures of all affected property owners, or their authorized representatives, are required on the application indicating consent to apply for the proposal. Evidence of ownership or authorization from the property owner to apply.

(2) A scaled site plan of both the existing and proposed property line configuration that contains the following:

~~(i) A vicinity map that clearly marks the site in relation to the nearest major streets, roads, and waterways in the area;~~

~~(ii) A map at a scale of not less than one (1) inch to fifty (50) feet which depicts the existing proposed property configuration, including all lot lines, dimensions, and lot area existing roads, easements, and structures within 10-feet of any proposed property line, with the distance between structures and the existing and proposed boundary lines.~~

~~(iii) The location and dimensions of all structures/improvements existing upon the affected lots and the distance between such structures/improvements and the existing and proposed boundary lines.~~

~~(iv) The location and dimensions of any easements within or adjacent to the affected lot(s).~~

~~(iiiv) The location, dimensions and names of all existing or platted street rights-of-way, whether public or private, within or adjacent to the affected lots.~~

ATTACHMENT A.1

(iii) The location of all existing and proposed water, sewer and storm drainage facilities, on-site wastewater disposal systems, drainfields, and wells.

(iv) The location of access to all affected lots.

(v) A north arrow and bar scale.

(3) A vicinity map that clearly marks the site in relation to the nearest major streets, roads, and waterways in the area.

~~(34)~~ The original legal descriptions of all affected properties, together with new separate legal descriptions for each parcel resulting from the adjustment.

~~(45)~~ All drawings and legal descriptions are required to be prepared, stamped and dated by a licensed land surveyor as set forth in BMC [20.12.070](#). The surveyor shall confirm the following:

(i) That the boundary line adjustment does not violate any covenants, conditions and restrictions (CCRs), deed restrictions, common spaces, easements, or development agreements pertaining to the affected properties; and

(ii) All persons with interest in the properties are represented in the request.

~~(56)~~ A title report prepared not more than sixty (60) calendar days prior to application submittal and prepared by a title company licensed in the State of Washington, may be required.

~~(67)~~ If an existing on-site sewage (septic tank) disposal system and/or well will continue to be used on an affected property after the boundary line adjustment, and/or if a new on-site sewage disposal system or well is proposed for an affected property where City code does not require connection to the City's municipal sewer and/or water system, the applicant shall provide written verification from the Kitsap Public Health District that the proposed lot is adequate to accommodate an on-site sewage disposal system and/or well.

(d) Decision Criteria. The following criteria shall be used to review and approve boundary line adjustments:

(1) The boundary line adjustment shall not result in the creation of any additional lot, tract, parcel, site, or division.

(2) The boundary line adjustment shall not be approved if it avoids public improvement requirements, such as utility or right-of-way improvements, that would be associated with a land division approval.

~~(3) The lots or parcels resulting after the boundary line adjustment shall meet all dimensional requirements specified for the applicable zone as set forth in this title, and other applicable regulations.~~

~~(4) No lot, use, or structure is made nonconforming or more nonconforming than that which existed at the time of application and is subject to the provisions of Chapter [20.54](#) BMC.~~

(3) The lots or parcels resulting from the boundary line adjustment shall be in conformance with all dimensional requirements specified for the applicable zone, or similar regulations.

(i) In the case of minor property line corrections, the Director may grant relief from dimensional requirements specified by the applicable zone, if the alteration is the minimum necessary to allow the subject property the same general rights

ATTACHMENT A.1

enjoyed by other property in the same area and zone; and the applicant demonstrates that other solutions or design options have been evaluated and rejected.

(54) Will not diminish or impair existing or future drainage, water supply, sanitary sewage disposal (including on-site sewage disposal) or legal.

(65) Shall not be reconfigured or adjusted which would render access for vehicles, utilities, fire protection, or existing easements impractical to serve their purpose or to allow maintenance/access.

(i) When an adjustment of boundary lines requires a modification of access, those modifications shall be approved by the City Engineer through a separate permit.

(76) Shall not violate or be inconsistent with any conditions of approval for a previously filed land use action, subdivision, short plat, or binding site plan deemed relevant by the Director.

(87) Shall not result in a lot having more than one land use designation and/or zoning; or result in being bisected by any special overlay.

(98) Shall not result in a lot, or lots, not wholly located within the City limits.

(109) Shall not involve lots that do not have a common boundary.

(110) Shall not result in a lot which would be so constrained by topography, critical areas or buffers, unusual shape, or other site conditions that a reasonable building site cannot be obtained except through a variance, reasonable use exemption from a critical areas permit, or other special exemption from the City's zoning, land use or critical area regulations.

(121) Shall not affect the boundaries of any lot, tract, parcel or division that is the subject of a current, unresolved City code enforcement action, code violation notice, or stop work notice; except as provided under circumstances where the Director, Hearing Examiner or judge deems a boundary line adjustment provides an appropriate resolution.

(1312) Any adjustment of boundary lines must be approved by the Department prior to the transfer of property ownership between adjacent legal lots.

~~(14) When an adjustment of boundary lines requires a modification of access, those modifications shall be approved by the City Engineer through a separate permit. Any adjusted lot shall contain no more than six (6) separate lot lines, which are straight lines except when an irregular line is caused by an existing right-of-way or existing lot line.~~

(e) Final Approval and Recording.

(1) Prior to recording, the applicant shall submit boundary line adjustment drawing(s) documents for approval by the City. All drawings and legal descriptions are required to be prepared, stamped and dated by a licensed land surveyor as set forth in BMC 20.12.070. The City will provide a decision in writing, to be provided to the Kitsap County Auditor's office. The following documents are required:

(i) Declaration of Boundary Line Adjustment. This documents shall provide the grantor, grantee, existing legal description(s), resulting legal description(s), and all assessor tax account numbers. Declaration of Boundary Line Adjustment shall include Kitsap County Auditor reference file to record-of-survey, and Kitsap County Auditor reference file to applicable deed of conveyance.

(ii) Record of Survey. Graphical depiction of the boundary line adjustment consistent with requirements of BMC 20.12.175(c)(2). Record of survey shall include Kitsap County Auditor reference file to Declaration of Boundary Line Adjustment, and Kitsap County Auditor reference file to applicable Deed of Conveyance.

(iii) Deed of Conveyance. For boundary line adjustments that involve multiply property owners, a Deed of Conveyance is required. Deed of Conveyance shall include Kitsap County Auditor reference file to Declaration of Boundary Line Adjustment, and Kitsap County Auditor reference file to Record of Survey.

~~(2) Prior to final approval, documentation authorizing the transfer of property ownership shall be placed on the original boundary line map along with the legal descriptions of those portions of land being transferred when lots are under separate ownership. Lot lines within lots under the same ownership will be adjusted upon the recording of the boundary line adjustment.~~

~~(3) A boundary line adjustment does not become effective until all documents are recorded with the Kitsap County Auditor. The boundary line adjustment shall be recorded within one hundred eighty (180) days of the notice of decision date or be null and void. The applicant shall be responsible for submitting all final documents for recording. The applicant shall provide the City the Auditor file number within seven (7) days of recording, and a copy of the recorded boundary line adjustment within thirty (30) days of recording. (Ord. 5422 §3, 2021)~~

CHAPTER 20.46 SPECIAL DEVELOPMENT STANDARDS

20.46.010 ACCESSORY DWELLING UNITS.

~~(g) One ADU is not required to provide an additional off-street parking space. The second ADU shall provide one (1) off-street parking space in addition to that which is required for the principal unit. The site must comply with Chapter 20.48 BMC Off-street Parking Requirements, specifically that the required parking spaces for the principal unit shall be provided and that all driveways and areas used for loading, parking, and maneuvering vehicles on the parcel shall have a paved surface.~~

20.46.030 HOME OCCUPATIONS.

~~(e) One (1) additional off-street parking space shall be provided in addition to the number of off-street parking spaces already required for the dwelling. The Director may waive this requirement if the home occupation involves internet services that do not require customers or deliveries at the residence.~~

20.46.070 ADAPTIVE REUSE OF COMMERCIAL BUILDINGS.

~~(2) Provision for off-street parking must be evaluated and, to the greatest extent possible, meet the parking demand for change of uses. The Director or Hearing Examiner may reduce the number of off-street parking spaces if commute trip reduction methods are employed and the adaptive reuse does not generate an increase in on-street parking demand.~~

20.46.090 TEMPORARY ENCAMPMENT PERMIT.

(d)(5) Parking Plan. The applicant shall develop a parking plan. The parking plan shall address the following:

- ~~(i) Adequate parking for the temporary encampment shall be provided so as not to reduce parking utilized by existing site uses;~~
- ~~(ii) The temporary encampment shall not displace the site's parking lot in such a way that the site no longer meets the minimum or required parking of the principal use as required by code or previous approvals;~~
- ~~(iii) An alternative parking plan may be approved by the Director if the parking plan can demonstrate how the existing use on the site and the encampment can provide off-site parking sufficient to have no off-site impact to the surrounding neighborhoods. This alternative parking plan will be not valid after the expiration of the temporary encampment;~~

20.46.170 COTTAGE HOUSING.

- ~~(i) One and one-half (1.5) parking spaces are required for each dwelling unit.~~

20.46.180 GROUP RESIDENTIAL CLASS I.

- ~~(3) Off-street parking shall be at a minimum of one (1) space per each employee during the peak shift, plus one (1) space per two (2) residents the facility will provide service to;~~
- ~~(4) If counseling services are provided to nonresidents, additional parking spaces are required at one (1) per three hundred (300) square feet of gross floor area used for counseling services;~~
- ~~(5) The number of required off-street parking spaces may be reduced, provided the applicant can demonstrate that parking will not spill over into nearby residential properties and any streets; and~~

20.46.190 SENIOR HOUSING COMPLEX.

- ~~(3) Off-street parking shall be a minimum one (1) space per dwelling;~~

20.46.200 MANUFACTURED HOME PARK.

- (8) Off-street parking spaces shall be provided in the following manner:
 - ~~(i) One (1) parking space per home site; plus~~
 - ~~(ii) One (1) parking space for each five (5) home sites for guest parking; plus~~
 - ~~(iii) Additional parking spaces to provide for the parking needs of offices, community buildings, recreational facilities, or other uses within the park that may be used by park residents or others;~~

20.46.220 FAMILY DAY CARE PROVIDER.

Consistent with RCW 36.70A.450, a Family Day Care Provider is a permitted use in all residential and mixed use zones. A Family Day Care Provider means a child care provider who regularly provides early childhood education and early learning services for not more than 12 children at any given time in the provider's home in the family living quarters except as provided in RCW 43.216.692. A Family Day Care Provider shall be processed as a Type I Permit. A Family Day Care Provider shall meet the following criteria:

ATTACHMENT A.1

- (a) Conforms with all applicable landscaping, development, design, and signage standards of the underlying zone;
- (b) Is certified by the Department of Children, Youth, and Families licensor as providing a safe passenger loading area;
- (c) Evidence is submitted demonstrating that written notification has been provided to immediately adjoining property owners, stating the intent to locate and maintain such a facility.

20.46.220 CO-LIVING HOUSING.

Co-living housing is a residential development with sleeping units that are independently rented and provide living and sleeping space, in which residents share kitchen facilities with residents of other units in the building. Co-living Housing shall be processed as a Type I Permit. Consistent with RCW 36.70A, Co-living Housing shall meet the following criteria:

- (a) Use. Co-living Housing meeting the following criteria shall be considered a permitted use:
 - (1) Lot Size. The minimum density of the underlying zone allows at least six multifamily residential units;
 - (2) Zone. The site is located in a residential or mixed-use zone;
- (b) Conforms with all applicable landscaping, development, design, and signage standards of the underlying zone;
- (c) Developed Density. Density requirements of the underlying zone shall apply, except that a sleeping unit in co-living housing shall count as one-quarter of a dwelling unit. This is a separate calculation than that used to determine a permitted use per BMC 20.46.220(a).

20.46.230 NEIGHBORHOOD-SCALE COMMERCIAL.

This code section relates to allowing small business establishments in residential zones. Neighborhood-Scale Commercial shall be processed as a Type II Permit. In all residential zones, Neighborhood-Scale Commercial may be permitted when conforming to the following criteria:

- (a) Use. The following uses are permitted per this section.
 - (1) Restaurant
 - (2) Drinking Place, only when also offering food
 - (3) Market/grocery store, only when limited to sale of food, beverages, and household items.
 - (4) Personal services
- (b) Zone. Conforms with all applicable development, design, and signage standards of the underlying zone;
- (c) Size. Area of the commercial use shall not exceed the following:
 - (1) 1,200 square feet of new construction.
 - (2) 2,500 square feet for existing buildings.
 - (3) Size limitations of this section do not apply to the High Density Residential zone or Adaptive Reuse Of Commercial Buildings per BMC 20.46.070.

- (4) Landscaping. Landscaping is provided consistent with the underlying zone and BMC 20.50.050.
- (5) Access. Site shall front on a public right-of-way and not be located on a dead-end street or cul-de-sac.

CHAPTER 20.48 OFF-STREET PARKING REQUIREMENTS

20.48.010 INTENT.

~~The intent of this chapter is to require off-street parking and loading facilities in proportion to the parking and loading demand of land uses while minimizing the amount of impervious surface where possible. The purpose of the regulations and design standards are this~~ chapter is to ensure the usefulness of parking and loading facilities, to protect the public safety, to mitigate adverse land use impacts and to protect the visual, water and air quality of the community. (Ord. 5301 §3 (Exh. B) (part), 2016: Ord. 4950 §8 (Exh. A) (part), 2005)

20.48.020 APPLICABILITY.

(a) General. The provisions of this chapter shall be applied at the time of erection of any building or structure, or at the time any principal building or structure is enlarged or increased in capacity unless provided for otherwise by law. Off-street parking refers to any parking, loading, motorcycle, bicycle, or other required by this chapter. This chapter shall be used in conjunction with specific off-street parking requirements set forth in this title. In cases of conflict, the most stringent requirement shall apply.

(b) Remodeled, Improved, or a Change of Use. When an existing structure is remodeled, improved, or a change of use or tenancy occurs on a legally established site, it shall be exempt from providing additional off-street parking conformance with this chapter; provided, that:

(1) The structure is not enlarged, extended, or structurally altered outside the existing building envelope in a manner that would require additional parking pursuant to this chapter. In the case of a structure expanding, the number of additional spaces shall be computed only to the extent of the enlargement, regardless of whether or not the number of previous existing spaces satisfies the requirements of this chapter.

(2) ~~In residential structures, a~~Alterations do not increase the number of dwelling units.

(3) Re-striping of existing parking may be required as a condition of permit approval. ~~The number of off street parking spaces is not decreased, except in cases where the resulting decrease in parking conforms to the minimum number required by this chapter.~~

(4) ~~This does not apply to adaptive reuses per BMC 20.46.070.~~

(c) Movement of Building. If a building is moved to a new lot, off-street parking spaces shall be provided in accordance with the requirements of this chapter. (Ord. 5301 §3 (Exh. B) (part), 2016: Ord. 4950 §8 (Exh. A) (part), 2005)

20.48.030 NONCONFORMITIES.

Any use which, on the effective date of the ordinance codified in this chapter or any amendments hereto, is nonconforming in terms of required off-street parking facilities may

continue in the same manner as if they were conforming unless any of the provisions set forth in BMC [20.48.020](#) would apply. When new development occurs, a maximum of six legally established nonconforming gravel surfacing parking spaces shall be permitted to remain in place; new development is not exempt from driveway standards per this Chapter or provisions per BMC 11.12. However, in no case shall nonconforming parking have its existing number of off-street parking spaces reduced. (Ord. 5301 §3 (Exh. B) (part), 2016: Ord. 4950 §8 (Exh. A) (part), 2005)

20.48.040 GENERAL REQUIREMENTS.

The following requirements are applicable to all zoning districts:

- (a) **Occupancy.** No building or structure shall be approved for occupancy unless adequate ~~off-street~~ parking facilities are provided consistent with the requirements of this chapter.
- (b) **Availability.** All required off-street parking spaces shall be made available and continuously maintained for the parking of residents, customers, patrons and employees of the related land uses and shall not be rented, leased or otherwise assigned to any person or organization not related to the principal or accessory land use.
- (c) **Plans Required.** No building permit shall be issued until the applicant has submitted satisfactory plans to the City showing how the off-street parking ~~and loading~~ facilities required by this chapter will be provided and maintained. Development of a parking lot without a building or other structure does not require a building permit, but does require ~~site plan review~~ a Site Development Permit in accordance with BMC 20.58.0890.
- (d) **Maintenance.** It shall be the responsibility of the property owner to ensure that parking areas and driveways are continually maintained in good condition and free of refuse and debris. All required landscaping shall be kept in a healthy condition and properly maintained.
- (e) **Nonparking Activities.** No business, temporary or permanent display, or other nonparking activity shall occur on any ~~required off-street parking area space~~ unless otherwise approved by the Department of Community Development.
- (f) **Parking Prohibited.** Parking is prohibited on landscaped or other areas not designed and approved for parking.
- (g) **Unobstructed Access and Circulation.**
 - (1) Unobstructed vehicular access to and from public or private streets shall be provided for all off-street parking spaces. Vehicular access drives shall be designed to ensure the safety of persons using such access or traveling on the public street.
 - (2) Required parking and all maneuver areas shall remain free of all obstructions.
 - (3) No parking space may block access to another parking space unless the City has approved a tandem parking design and valet parking with a tenant is made available during hours of operation.
 - (4) Except for single-family and duplex structures on individual lots, parking spaces shall be designed to prevent the backing of vehicles onto a public right-of-way, or a private street other than an alley.
- (h) **Shared Driveways.** Shared driveways are encouraged for abutting lots whenever practical. Approval shall include easements granting access to all property owners using the shared driveway. (Ord. 5301 §3 (Exh. B) (part), 2016: Ord. 4950 §8 (Exh. A) (part), 2005)

20.48.050 MODIFICATIONS TO PARKING DEVELOPMENT STANDARDS.

The parking development standards included in BMC [20.48.060](#) and [20.48.080](#) are base standards. Individual zone chapters may provide additional parking standards, or departures from these base standards. In cases of conflict, parking development standards in individual zone chapters shall supersede. ~~Further reductions from base parking standards may be allowable per BMC [20.48.100](#).~~ (Ord. 5301 §3 (Exh. B) (part), 2016; Ord. 4950 §8 (Exh. A) (part), 2005)

20.48.060 RESIDENTIAL PARKING DEVELOPMENT STANDARDS.

The following provisions apply to off-street parking spaces for all single-family, duplex, and accessory dwelling unit residential development:

- (a) Surface. Driveways and areas used for loading, parking and maneuvering motorized vehicles shall have a paved surface. Consistent with RCW 36.70A, residential parking spaces that consist of grass block pavers may count toward minimum parking requirements.
- (b) Gravel Surface Driveway. A gravel surface driveway may be allowed for a single-family residence for that portion of the driveway that is more than one hundred (100) feet from the lot line where access is provided. Any driveway approved for a gravel surface shall include a paved apron in front of the garage automobile door entrance extending a minimum depth of eighteen (18) feet and at least the width of the garage door.
- (c) Side Yard Setback. Minimum three (3) feet. When parking is located in an approved driveway, the setback for parking may be reduced to zero (0) feet.
- (d) Rear Yard Setback. Minimum three (3) feet. When parking is located in an approved driveway, the setback for parking may be reduced to zero (0) feet.
- (e) Alley Setback. Zero (0) feet.
- (f) Front Yard Setback. No parking shall be located within the front yard setback area of the zone, except within paved driveways, unless allowed otherwise by law.
- (g) Stall Dimensions.
 - (1) ~~Parking stalls for a single-family dwelling development subject to BMC [20.48.060](#)~~ shall have a minimum width of eight (8) feet and depth of eighteen (18) feet.
 - (2) ~~Structures containing parking facilities for two (2) three (3) or more dwellings shall comply with the design standards prescribed in BMC [20.48.080](#). These design standards may be modified to allow stacked parking spaces for a residential structure containing up to four (4) dwellings, provided the parking spaces comply with setbacks.~~
- (h) Driveways.
 - (1) A driveway may be located within any setback area for a residential use, provided it complies with all applicable city street and engineering standards established in BMC Title [11](#).
 - (2) The driveway shall not be less than eight (8) feet in width and not more than twenty (20) feet in width within the front yard setback. The Director may approve exceptions to the maximum driveway width when necessary for compliance with the Americans with Disabilities Act (ADA) standards.

ATTACHMENT A.1

(i) Required Parking Spaces. Development is exempt from providing automobile off-street parking spaces, except as otherwise required for conformance with the Federal Americans with Disabilities Act (ADA), the State of Washington, and section BMC 20.48.060(i)(1). The number of off-street parking spaces shall be provided in accordance with the use and the following corresponding standards, except as modified per BMC ~~20.48.050~~:

- (1) Single-family homes, located in the Low Density Residential zone shall provide 1 parking spaces per residential unit, except for the following exempted housing types:
 - (i) Residences under 1,200 square feet;
 - (ii) Affordable housing as defined per RCW 36.70A.030;
 - (iii) Senior housing for Eligible Persons as defined per RCW 74.38.020(5);
 - (iv) A building undergoing a change of use from a nonresidential to a residential use.

Type of Use	Number of Bedrooms	Minimum Number of Parking Spaces
(1) ADU	-	See BMC 20.46.010
(2) Single-unit residential	All	2.0 per dwelling
(3) Two-unit residential	All	2.0 per dwelling
(4) Multi-unit residential	1 or less	1.5 per dwelling
(5) Multi-unit residential	2	1.75 per dwelling
(6) Multi-unit residential	3 or more	2.0 per dwelling
(7) Multi-unit residential located within a center	All	1.0 per dwelling
(8) Group residential facility, Class I	-	2.0 plus 1.0 for each bedroom beyond the first 4 plus 1.0 for each 300 square feet of gross floor area used for counseling
- Group residential home	-	
- Adult family home	-	
(9) Senior housing complex	3 or more	1.0 per dwelling, plus access to alternative transportation such as public transit or on-site shuttle services to access daily goods and services shall be provided

(Ord. 5301 §3 (Exh. B) (part), 2016: Ord. 5118 §2, 2010: Ord. 4950 §8 (Exh. A) (part), 2005)

20.48.080 NONRESIDENTIAL PARKING DEVELOPMENT STANDARDS.

The following provisions apply to off-street parking spaces for all nonresidential developments, other uses not addressed per BMC 20.48.060, and parking lots:

- (a) Surface. Driveways and areas used for loading, parking and maneuvering motorized vehicles shall have a paved surface. Consistent with RCW 36.70A, residential parking spaces that consist of grass block pavers may count toward minimum parking requirements.
- (b) Side and Rear Yard Setbacks. Parking spaces shall be set back a minimum of five (5) feet from the side and rear lot lines unless adjacent to the low density residential zone in which case a minimum of ten (10) feet of visual screening is required pursuant to BMC 20.50.050. Projects which are not adjacent to low density residential may reduce setbacks to zero (0) if the following are incorporated into the design of the parking area:
 - (1) A solid fence or wall is provided; and
 - (2) Landscaped islands at least forty-five (45) square feet of area are provided in a manner that breaks up parking spaces adjacent to the lot line into groups of not more than six (6) spaces each.
- (c) Front Yard Setbacks. Surface Parking spaces shall be set back a minimum of ten (10) feet from the front lot line, or the front yard setback depth established in the zone, whichever is greater.
- (d) Parking Setbacks in Structures. When parking is incorporated within a structure, the parking shall comply with the setbacks of the zone applicable to the structure.
- (e) Stall and Aisle Requirements. Parking facilities for required parking shall meet the minimum stall and aisle dimensions set forth in Figure 20.48(a).
- (f) Modifications to Parking Stall Requirements. The standards set forth in subsection (e) of this section may be modified to allow for a vehicle overhang of up to two and one-half (2.5) feet, provided:
 - (1) Wheel stops are attached to the ground or pavement if curbs are not provided;
 - (2) The vehicle overhang would not extend beyond the lot line or into front yard setback areas;
 - (3) Walkways maintain a minimum three (3) foot wide (four (4) feet for handicap access routes) unimpeded passage for pedestrian passage beyond the two and one-half (2.5) foot overhang.
- (g) Driveways. The width of nonresidential driveways shall be based on the location, intended usage, and other factors and shall be in accordance with state and City engineering standards.
- (h) Internal Parking Lot Design. Parking facilities shall incorporate the following features to provide safe and efficient circulation for vehicles and pedestrians:
 - (1) Standard traffic control signs and devices to direct traffic;
 - (2) Signs, crosswalks, raised pedestrian walkways and night lighting to provide for safe pedestrian movement;
 - (3) Landscaped islands, raised curbs, and striping to define parking lot entrances, end of parking aisles and the patterns of internal circulation; and
 - (4) Overall design coordination and adherence to all applicable design standards.

ATTACHMENT A.1

(i) Landscaping. Landscaping shall be provided in accordance with the requirements prescribed in Chapter [20.50](#) BMC.

(j) Lighting. Parking areas shall be lighted for the safety of pedestrians and vehicle circulation on the site in the following manner:

~~(1) Lights are hooded or beamed so that there is no undesirable glare directed onto any adjacent streets or property; and~~

~~(12) Lighting shall be designed adequately to illuminate the parking area and also demonstrate consistency with BMC 20.44.110 while preventing the direct illumination of adjacent properties.~~

(k) Signage and Striping. Directional signs and pavement markings shall be used to control vehicular movement within parking areas and to mark spaces.

(1) The locations of parking spaces shall be clearly indicated by curb markings, wheel stops, bollards, or other means.

(2) Incidental signs within parking lots shall be limited to two (2) square feet in size.

(3) No signs other than those indicating entrances, exits, name of the lot or the establishment to which the lot is accessory, conditions of use, or identification of disabled parking spaces shall be erected.

(4) Signage for handicap spaces shall comply with the requirements prescribed in RCW [70.92.120](#).

(l) Required Parking Spaces. ~~Development is exempt from providing automobile off-street parking spaces, except as otherwise required for conformance with the Federal Americans with Disabilities Act (ADA) or the State of Washington. The number of off-street parking spaces shall be provided in accordance with the use and following corresponding standards except as modified per BMC [20.48.050](#):~~

Category of Land Use	Minimum Number of Parking Spaces
BUSINESS/COMMERCIAL:	-
(1) Barber shop/beauty salon	1 per 75 sq. ft. gross floor area (g.f.a.)
(2) Bank/financial	1 per 350 sq. ft. g.f.a.
(3) Laundry (self-service)	1 per 4 washing machines, with a minimum of 5 required
(4) Office	1 per 300 sq. ft. g.f.a.
(5) Retail store less than 15,000 sq. ft.	1 per 300 sq. ft. g.f.a.
(6) Retail store 15,000 sq. ft. or larger	1 per 250 sq. ft. g.f.a.
(7) Personal service shop (tanning, shoe repair, dry cleaner)	1 per 250 sq. ft. g.f.a., with a minimum of 2 required
(8) Plumbing, heating, electrical and building supplies	1 per 600 sq. ft. g.f.a.
(9) Convenience market/food store (retail)/market and supermarket	1 per 250 sq. ft. g.f.a., with a minimum of 6 required

ATTACHMENT A.1

Category of Land Use	Minimum Number of Parking Spaces
(10) Shopping center*	4.0 per 1,000 square feet of leaseable g.f.a.
*—A shopping center is a contiguous collection of retail businesses under one ownership or common management located in a building or set of buildings.	
EATING/DRINKING ESTABLISHMENTS: -	
(1) Restaurant/cocktail lounge/tavern less than 4,000 sq. ft.	1 per 150 sq. ft. g.f.a.
(2) Restaurant/cocktail lounge/tavern 4,000 sq. ft. or greater	20 plus 1 per 100 sq. ft. g.f.a. greater than 4,000 sq. ft.
ENTERTAINMENT: -	
(1) Bowling alley	5 per lane
(2) Club, lodge	1 per 4 fixed seats, plus 1 per 50 sq. ft. gross floor area of assembly area without fixed seating
(3) Dance hall	1 per 100 sq. ft. g.f.a.
(4) Pool hall	1 per 100 sq. ft. g.f.a.
(5) Theater	1 per 4 fixed seats
(6) Video game arcade	15 per 1,000 sq. ft. g.f.a.
INDUSTRIAL: -	
(1) Industrial establishment/research and development	1 per 600 sq. ft. g.f.a.
(2) Mini-warehouse	1 per 3,000 sq. ft. g.f.a., plus 1 loading space per 10,000 sq. ft. g.f.a., plus 1 per 300 sq. ft. g.f.a. for office area
(3) Wholesale establishment/warehouse, trucking and freight terminal	1 per 2,000 sq. ft. g.f.a. up to 20,000 sq. ft. (3 space minimum), plus 1 per 2,500 sq. ft. g.f.a. 20,001 to 100,000 sq. ft. (10 space minimum), plus 1 per 3,000 sq. ft. greater than 100,000 sq. ft. (40 spaces minimum)
LODGING: -	
(1) Bed and breakfast	1 per guest room plus 1 for the resident household
(2) Hotel	1 per 2 guest rooms, plus 1 per 200 sq. ft. g.f.a. of common areas
(3) Motel	1 per 2 guest rooms, plus 1 per 200 sq. ft. g.f.a. of common area plus 1 for manager

ATTACHMENT A.1

Category of Land Use

(4) Travel trailer park/tourist court

Minimum Number of Parking Spaces

1 per trailer site, plus 1 per 10 sites located in a secure storage area, plus 1 per 5 sites for guest parking, plus other parking for office or community buildings

MEDICAL/DENTAL:

(1) Medical/dental office

1 per 150 sq. ft. g.f.a.

(2) Hospital/medical centers

1.8 per overnight bed based on state license

(3) Nursing home/health institution/convalescent home

1 per 600 sq. ft. g.f.a.

(4) Veterinarian/animal hospital

1 per 200 sq. ft. g.f.a.

RECREATIONAL:

(1) Health club/figure salon/physical fitness center

10 per 1,000 sq. ft. g.f.a.

(2) Parks/playground

Based on anticipated parking demand

(3) Racquet/handball

2 per each court

(4) Swimming facility

5, plus 1 per 75 sq. ft. of gross surface water area

PUBLIC:

(1) Library/museum

1 per 400 sq. ft. g.f.a.

(2) Places of assembly (arenas, churches, auditoriums, stadiums, sports arenas, etc.)

1 per 4 fixed seats (20 inches of a bench or pew = 1 seat), plus 1 per 50 sq. ft. g.f.a. of assembly without seats

EDUCATIONAL:

(1) Nursery school/day care

1 per 5 children, plus 2 loading/unloading spaces (12 or fewer children are exempt from the loading/unloading)

(2) School, business and trade

6 per classroom, and/or 1 per 200 sq. ft. g.f.a. of administrative office, and/or teaching lab or shop

(3) School, college or university

2 per 1,000 sq. ft. g.f.a. used for academics, plus 1 per each 4 student rooming units, plus additional space as required for nonacademics

(4) School, K through 9th grade

2 per classroom, plus 1 per 200 sq. ft. g.f.a. of administrative office

ATTACHMENT A.1

Category of Land Use	Minimum Number of Parking Spaces
(5) School, 10th through 12th grade	1 per each 10 students for which building is designed, plus 1 per classroom, plus 1 per 200 sq. ft. g.f.a. of administrative office
AUTOMOBILE:	-
(1) Automobile service	1 per 200 sq. ft. g.f.a., plus 1 per service bay (bay itself is not a space), with a minimum of 5 required
(2) Motor vehicle/vessel sales lots	1 per 3,000 square feet of retail indoor or outdoor sales area in addition to parking requirements for the building or structure
(3) Motorcycle/small engine repair	1 per 400 sq. ft. g.f.a.
(4) Shop/store for sales and service of machinery	1 per 600 sq. ft. g.f.a.
OTHERS:	-
(1) Garden nursery	1 per 400 sq. ft. g.f.a. of retail area, plus 1 per 500 sq. ft. g.f.a. of building and outside display area (wholesale)
(2) Marina and moorage facility	1 per 600 sq. ft. g.f.a.
(3) Mortuary/funeral home	1 per 75 sq. ft. of assembly area
(4) Uncovered storage area	1 per 2,000 sq. ft., plus 1 per each vehicle anticipated to be parked overnight on a regular basis, plus 1 per 300 sq. ft. g.f.a. for office

(Ord. 5301 §3 (Exh. B) (part), 2016; Ord. 5046 §3, 2008; Ord. 4950 §8 (Exh. A) (part), 2005)

20.48.100-PARKING REDUCTION REPEALED.

Modifications to the requirements of this chapter may be granted for the following:

(a) For a modification to the number of off-street parking spaces required by this title, a decrease in the required number of spaces may be granted if the number of spaces proposed is documented by an adequate and thorough parking-demand-and-utilization-study to sufficiently serve the use with appropriate off-street parking. The study shall be prepared by a licensed transportation engineer or other qualified professional with parking demand expertise, and shall analyze the operational characteristics of the proposed use which justify a parking reduction. Approval of a parking reduction shall be solely at the discretion of the City. The following reductions do not require the above mentioned report:

(1) Covered Bicycle Storage. If covered and secured bicycle storage is provided on-site, a credit towards parking requirements at a ratio of one (1) less parking stall per six (6) bicycle spaces will be granted. The Director may increase credits according to size of development and anticipated pedestrian and bicycle activity and proximity to transit

facilities. A maximum reduction of five (5) percent of required parking stalls may be granted. If a reduction of five (5) or more stalls is granted, then changing facilities including showers and lockers shall be required.

(2) ~~Transit Improvements.~~ For new residential development consisting of ten (10) or more dwelling units, or commercial projects consisting of fifty (50) or more employees, and located within one thousand five hundred (1,500) feet of an existing or planned transit facility, a reduction in parking may be requested for up to fifteen (15) percent of required spaces if improvements to the local transit system are made by the applicant. Any such plan requires the approval of Kitsap Transit and the City.

(3) ~~Designated Land Use Centers.~~ If a change of use occurs on a property within a center zone designation, resulting in a significant increase in the parking requirement, the Director may reduce the minimum number of required parking spaces by up to ten (10), provided:

(i) ~~Sites containing multiple buildings and/or uses shall have the reduction applied to the entire site and not to individual uses;~~

(ii) ~~The number of existing off-street parking spaces is not reduced.~~

(4) ~~Group Residential Facility - Class I, Adult Family Home, and Group Residential Home.~~ The Director may authorize a reduction in the number of required parking spaces for a group residential facility - Class I, adult family home, or group residential home if there is a clear indication that the requirements of this chapter are excessive due to such factors as the resident's age, ability to drive, or mental or physical abilities and disabilities. (Ord. 5301 §3 (Exh. B) (part), 2016; Ord. 4950 §8 (Exh. A) (part), 2005)

20.48.120 UNLISTED USES REPEALED.

Land uses not specifically listed in BMC ~~20.48.060(i)~~ and ~~20.48.080(l)~~ shall have their off-street parking requirements determined in the following manner:

(a) ~~A comparison is made of the unlisted use to a listed use and the Director determines that the parking demand between the two (2) uses is similar. (Ord. 5301 §3 (Exh. B) (part), 2016; Ord. 4950 §8 (Exh. A) (part), 2005)~~

20.48.140 ACCESSIBLE PARKING.

Where parking is provided, accessible parking spaces shall be provided that comply with the requirements in Chapter [17.04](#) BMC, City Building Code (IBC Chapter 11). Accessible parking spaces shall be located on the shortest accessible route of travel from adjacent parking to an accessible building entrance. (Ord. 5301 §3 (Exh. B) (part), 2016; Ord. 4950 §8 (Exh. A) (part), 2005)

20.48.150 ELECTRIC VEHICLE CHARGING STATIONS.

For uses in allowed zones and where parking is provided, electric vehicle charging stations are encouraged and shall comply with the City Building Code and Washington State Electrical Code (Chapter [296-46B](#) WAC). (Ord. 5301 §3 (Exh. B) (part), 2016; Ord. 5249 §5, 2014)

20.48.160 ~~LOADING/UNLOADING SPACES REPEALED.~~

~~(a) Standards. Off-street loading spaces shall be provided in accordance with the following standards for all new nonresidential construction or additions having a floor area greater than one thousand (1,000) square feet of gross floor area that is engaged in retail, manufacturing, wholesale or storage activities, but excluding self-service storage facilities, which requires the delivery of merchandise or materials by trucks:~~

Total Gross Floor Area	Number of Loading Spaces Required
(1) 1,000 to 20,000 square feet	1
(2) 20,001 to 50,000 square feet	2
(3) 50,001 to 100,000 square feet	3
(4) Greater than 100,000 square feet	4

~~(b) Standards. Off-street loading spaces shall be provided in accordance with the following standards for all new nonresidential construction or additions having a floor area greater than ten thousand (10,000) square feet of gross floor area that is engaged in hotel, office, restaurant or similar use in accordance, which requires the delivery of merchandise or materials by trucks:~~

Total Gross Floor Area	Number of Loading Spaces Required
(1) 10,000 to 50,000 square feet	1
(2) 50,001 to 100,000 square feet	2
(3) Greater than 100,000 square feet	3

~~(c) Loading/Unloading Zone Development Standards:~~

~~(1) Design. Off-street loading spaces shall be at least ten (10) feet in width and have a total area of not less than two hundred fifty (250) square feet.~~

ATTACHMENT A.1

~~(2) Accessibility. With the exception of gates and security fencing, loading spaces shall be accessible at all times from a street, alley, or driveway and shall not be obstructed by other loading spaces, trash receptacles, outdoor storage, or other obstacles.~~

~~(3) Obstructions. Loading functions shall not interfere with any emergency access, fire exit, or required vehicle parking spaces.~~

~~(d) Modifications:~~

~~(1) If the applicant can demonstrate that deliveries are only made during nighttime or other hours when customers are not present, the loading function may be accommodated in combination with required customer/employee parking.~~

~~(2) If the applicant can demonstrate that all deliveries are made by trucks with a gross weight of twenty-six thousand (26,000) pounds or less, the number of required loading/unloading spaces may be reduced by one (1) if a load/unload zone located within the public right-of-way fronting the property is approved by the Public Works Department. (Ord. 5301 §3 (Exh. B) (part), 2016; Ord. 4950 §8 (Exh. A) (part), 2005)~~

20.48.180 BICYCLE FACILITIES OTHER REQUIRED PARKING SPACES.

All uses subject to BMC 20.48.080 nonresidential uses and residential developments of six (6) or more dwellings shall provide parking spaces for the following:

(1) Residential uses shall provide 1 bicycle parking space for each residential unit. All residential bicycle parking required by this section shall be provided within a secured building. Bicycle parking may be located within a shared bicycle storage room, indoor secured bike cage, within individual residential units, or other configuration approved by the Director.

(2) Nonresidential uses shall provide 1 bicycle parking space per 4,000 square feet of gross floor area. Bicycle parking for nonresidential uses may be located exterior to the building.

(3) Bicycle parking stalls shall provide adequate space for maneuvering and storage.

Examples

of dimensional standards are below:

i. The standard required bicycle space is 2-feet wide, 6-feet long, and 3-feet 4-inches tall.

ii. There must be at least 5-feet space adjacent to all bicycle parking spaces to allow room for bicycle maneuvering.

iii. A wall clearance of 2-feet 6-inches must be provided.

iv. Alternative bicycle parking configurations may be approved by the Director approval, provided the applicant demonstrates dimensions listed in this section are not feasible.

~~(a) Motorcycle Spaces. Motorcycle spaces shall be provided at one (1) per twenty-five (25) of the required automobile spaces with at least one (1) motorcycle space provided.~~

~~(1) Automobile parking requirements may be reduced one (1) space for every three (3) motorcycle spaces provided, up to a maximum five (5) percent of the total required automobile spaces.~~

ATTACHMENT A.1

(2) ~~Each motorcycle space shall be no smaller than four (4) feet in width and eight (8) feet in depth.~~

(3) ~~Motorcycle spaces shall be located according to the same criteria and standards that are applicable to automobile parking spaces.~~

(4) ~~The Director may exempt those nonresidential uses that would not normally have motorcycle-riding clientele (such as warehouses, storage facilities, automobile services, etc.) from motorcycle parking requirements.~~

(b) ~~Bicycle Facilities. Bicycle parking spaces shall be provided at ten (10) percent of the required automobile spaces.~~

(1) ~~The minimum number of required bicycle spaces for schools of all types except higher education institutions shall be calculated at the rate of one (1) space per twenty (20) students, based on the facility's designed capacity.~~

(2) ~~The Director may exempt those nonresidential uses that would not normally have bicycle-riding clientele (such as self-service businesses, automobile services, gas stations, etc.) from the bicycle parking requirements.~~

(3) ~~Bicycle facilities shall be firmly attached to the ground, located near the building entrance, and designed to allow both the frame and wheels to be securely locked to the structure. (Ord. 5301 §3 (Exh. B) (part), 2016; Ord. 4950 §8 (Exh. A) (part), 2005)~~

20.48.200 RENT OF RESIDENTIAL PARKING.

(a) The rental, lease or other use of automobile parking spaces for commercial parking purposes within a residential zone is prohibited.

(b) The rental, lease or other use of parking spaces associated with a residential use for commercial parking, whether they are required spaces or not, is prohibited. (Ord. 5301 §3 (Exh. B) (part), 2016; Ord. 4950 §8 (Exh. A) (part), 2005)

20.48.220 SATELLITE PARKING REPEALED.

~~Off-street parking requirements may be partially or completely satisfied with the approval of a satellite parking facility. A satellite parking facility is approved by site plan review as prescribed in BMC 20.58.080. A satellite parking facility may be approved, provided:~~

~~(a) The bicycle parking facility is located within four hundred (400) feet of the property containing the associated use or building.~~

~~(b) The parking facility is not located on property zoned for low density residential. The Director may waive the zone restriction for a religious worship facility, public school or public use if:~~

~~(1) The parking facility is located on a lot abutting the principal use; or~~

~~(2) The parking facility is located within one hundred (100) feet on a lot where a street (public or private) or alley would separate it from the principal use.~~

~~(c) A notice to title, approved by the City Attorney and executed by affected parties to set forth clearly the terms of the parking agreement, shall be recorded with the County Auditor. A copy of the recorded agreement shall be submitted to the Department.~~

~~(d) The satellite parking facility shall meet the requirements of this chapter.~~

(e) A limit on the time of day that the satellite parking facility is used may be placed as a condition of its approval if deemed necessary by the Director for the protection of adjoining areas.

20.48.240 JOINT USE OF PARKING FACILITIES REPEALED.

Joint use parking facilities provide for the sharing of parking facilities between different uses may be granted under the following conditions:

- (a) The principal building or use shall be located within a radius of eight hundred (800) feet of the joint parking facilities.
- (b) The principal building or use shall not be separated from its parking by an arterial street unless it is within one (1) block of a signalized intersection.
- (c) The applicant shall demonstrate there will be no substantial conflict between the two (2) uses regarding hours of operation for which joint use of parking facilities is proposed.
- (d) A notice to title, approved by the City Attorney and executed by affected parties to set forth clearly the terms of the parking agreement, shall be recorded with the County Auditor. A copy of the recorded agreement shall be submitted to the Department.
- (e) The right to occupy the primary premises shall terminate if the joint use parking facilities that are necessary and were approved cease to be available. (Ord. 5301 §3 (Exh. B) (part), 2016; Ord. 4950 §8 (Exh. A) (part), 2005)

20.48.260 DRIVE-THROUGH STACKING REQUIREMENTS.

Drive-through stacking requirements can be found in BMC [20.44.120](#). (Ord. 5301 §3 (Exh. B) (part), 2016; Ord. 5222 §5, 2013; Ord. 4971 §10, 2006; Ord. 4950 §8 (Exh. A) (part), 2005)

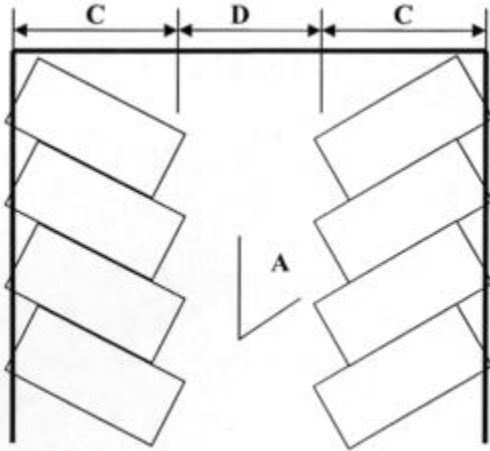
Figure 20.48(a)

Parking Minimum Design Requirements

Parking Angle	Standards			
	Minimum Stall Width	Minimum Stall Depth	Minimum Aisle Width	
			One-way	Two-way
0°	22'0"	8'0"	10'0"	18'0"
30°	8'6"	17'0"	12'0"	20'0"
45°	8'6"	17'4"	12'3"	20'0"
50°	8'6"	18'0"	12'9"	20'0"
55°	8'6"	18'6"	13'3"	20'0"
60°	8'6"	18'10"	14'4"	20'0"
65°	8'6"	19'0"	15'2"	21'0"
70°	8'6"	19'2"	16'0"	21'6"
75°	8'6"	19'0"	17'6"	22'0"
90°	8'6"	17'11"	22'6"	22'6"

Parking Minimum Design Requirements

Parking Angle	Standards			
	Minimum Stall Width	Minimum Stall Depth	Minimum Aisle Width	
			One-way	Two-way
A	B	C	D	D



(Ord. 5301 §3 (Exh. B) (part), 2016: Ord. 4950 §8 (Exh. A) (part), 2005)

CHAPTER 20.54 NONCONFORMING PROVISIONS

20.54.060 NONCONFORMING USES.

(c) Expansion. A nonconforming use may not be expanded or enlarged, except under one (1) of following circumstances:

- (1) Alterations are permitted, provided it is within the existing physical space of the building or use.
- (2) Residential dwellings may have the building area expanded if the number of dwelling units is not increased above or below the requirements of the zone in which it is located, there is no decrease in the number of off-street parking spaces below the minimum requirements and the addition complies with all zoning requirements.
- (3) The acquisition of additional accessory off-street parking is not an expansion of a nonconforming use.

20.54.080 NONCONFORMING LANDSCAPING.

Uses which were lawfully established which do not conform to landscaping requirements need not provide additional landscaping, except:

- (a) Expansion of a Structure. If an existing structure or use is expanded, additional landscaping shall be provided in accordance with the requirements of Chapter 20.50 BMC. The amount of additional landscaping shall be computed only to the extent of the expansion regardless of whether existing landscaping complies with the requirements.

(b) Change of Use. A change to a use that requires greater amounts of landscaping than the former use shall provide landscaping in accordance with the requirements of Chapter 20.50 BMC. ~~The Director may modify the amount of landscaping required if providing the required landscaping would result in a reduction of the parking below the minimum required by Chapter 20.48 BMC.~~

(c) Parking Lot. When an existing unpaved parking area is paved, it shall be landscaped in accordance with the requirements of Chapter 20.50 BMC. ~~The Director may modify the amount of landscaping required if the landscaping would result in a reduction of the parking below the minimum required by Chapter 20.48 BMC.~~

20.54.120 NEW HOUSING IN NONCONFORMING BUILDINGS.

The following provisions are intended to comply with State of Washington requirements per RCW 35.21.990, to allow additional housing units within existing buildings. For the purpose of this section, "existing building" means a building that received a certificate of occupancy at least three years prior to the permit application to add housing units. Nothing in this section requires a city to approve a building permit application in cases in which the building cannot satisfy life safety standards. Consistent with RCW 35.21.990, the following shall apply:

(a) Nonconformities. Development shall comply with all City regulatory criteria, except as otherwise exempt from Building Code requirements per RCW 35.21.990(g), existing nonconformities per RCW 35.21.990(h), and transportation concurrency studies RCW 35.21.990(i).

(b) Density. When constructed entirely within an existing building envelope, in a building located within a zone that permits multifamily housing, a 50 percent density increase is permitted above that designated in the underlying zone. To ensure community health and safety, densities per BMC 20.54.120(b) do not apply in any of the following instances:

(1) When Building Code standards, Fire Code standards, or other life safety standards cannot be met;

(2) When a building is located on portions of a lot, parcel, or tract designated with critical areas under RCW 36.70A.170, or their buffers as required by RCW 36.70A.170, or within the Shoreline Jurisdiction.

(c) Ground Floor Commercial. In zones requiring mixed uses, housing units shall not occupy the ground floor area, except as otherwise permitted by the underlying zone.

CHAPTER 20.58 LAND USE PERMITS

20.58.060 RESIDENTIAL CLUSTER DEVELOPMENT.

(a) Intent. The intent of the residential cluster development (RCD) is to accommodate urban densities of the underlying zoning district while allowing residential development to utilize less land area. These provisions aim to allow greater flexibility in the design of subdivisions to ensure development is in harmony with the natural characteristics onsite and to preserve features such as critical areas, open space, recreation areas, or scenic vistas.

ATTACHMENT A.1

(b) Applicability. A RCD may be applied to all subdivisions as permitted in Chapter 20.12 BMC, Land Division.

(c) Procedures.

(1) A RCD shall be processed in coordination with a subdivision application and will follow the permitting procedures established in Chapter 20.12 BMC, Land Division.

(2) In addition to the notification requirements prescribed in BMC 20.02.100, the notice of application shall be mailed to all property owners within three hundred (300) feet of the property.

(d) Development Standards. Modification may be allowed to the underlying zone for which a RCD applies as follows:

(1) Density. The minimum and maximum density of the underlying zone shall apply, however, unbuildable critical areas as defined in Chapter 20.14 BMC may be included in the area for calculating density; however, land used for right-of-way may not.

~~(2) Minimum Lot Size. Lot size may be reduced to two thousand five hundred (2,500) square feet, provided all lots are buildable:~~

~~(i) Reductions below two thousand five hundred (2,500) square feet will only be permitted if the applicant provides an additional ten (10) percent of permanently protected open space as required by subsection (d)(8) of this section.~~

~~(23) Setbacks.~~

~~(i) For the purposes of this section perimeter setbacks shall be defined as the exterior boundary of the entire property to be subdivided. Structures and parking areas shall be set back a minimum of ten (10) feet from all perimeter property lines.~~

~~(ii) All other setbacks may be modified to encourage innovative site design.~~

~~(34) Lot Width. Deviations are permitted.~~

(45) Development Coverage. The development coverage is limited to fifty (50) percent of the total site area including open space.

~~(56) Parking. Two parking spaces are required for each primary residence. All deviations from the standard parking requirements shall be accompanied by a detailed analysis showing how alternative parking will provide better site design and will provide clear benefits to the residents. Deviations are permitted as follows:~~

~~(i) Setbacks may be modified;~~

~~(ii) Parking may be applied to the entire site rather than to individual lots;~~

~~(iii) Parking tracts shall be placed in easily accessible locations;~~

~~(iv) Designated parking spaces should be clearly marked.~~

~~(67) Housing Type. Attached and detached single-family homes, and townhomes, and other residential uses permitted by the underlying zone are allowed as primary residential structures. Accessory dwelling units are permitted pursuant to BMC 20.46.010.~~

~~(78) Open Space Designation and Preservation.~~

~~(i) Each RCD shall provide not less than fifteen twenty (1520) percent of the gross site area for common open space which shall be primarily concentrated in large areas. When these areas are designated as critical areas they should be designed to provide connectivity for habitat functions;~~

ATTACHMENT A.1

- (ii) Open space areas shall be located on separate tract(s), and shall be developed for passive or active recreational uses or set aside to preserve critical areas as defined in the critical area regulations in Chapter 20.14 BMC;
- (iii) Parking areas, rights-of-way, driveways, and yards within individual lots shall not be included in common open space;
- (iv) Facilities and other improvements that enhance recreational use(s) may be located in an open space area (provided they are not prohibited by other sections of the BMC). Such facilities can include, but are not limited to, fields, picnic areas, playgrounds, and athletic courts.
- (v) Open space areas shall be restricted in perpetuity from further subdivision and/or land development. This restriction shall be noted on the face of the plat.
- (vi) Open Space Plan. For all designated open space areas the proposal shall include an open space plan. This plan is to ensure the open space is maintained and preserved in perpetuity. Open space requirements established in the critical areas regulations (Chapter 20.14 BMC) shall be met for all designated critical areas. All open space areas excluding critical areas shall comply with the following:
 - (A) Identify all proposed improvements such as public facilities, proposed vegetation, and existing vegetation to remain; and
 - (B) The plan must comply with the requirements of the landscaping chapter, found in Chapter 20.50 BMC; and
 - (C) The plan shall outline maintenance responsibilities in a format acceptable to the Director; and
 - (D) The plan shall include a notice to title approved by the City and recorded with the County Auditor. This notice shall include provisions for the designation and retention of open space, and provisions for permanent maintenance of the open space and/or commonly owned facilities.
- (e) Criteria for Approval. The decision-making authority may grant a RCD only if it is found that:
 - (1) A RCD shall be approved with the approval of a subdivision. The RCD shall follow the same approvals and timelines as the concurrent subdivision application pursuant to Chapter 20.12 BMC, Land Division. (Ord. 5506 §14, 2024; Ord. 5301 §3 (Exh. B) (part), 2016; Ord. 5063 §3, 2008; Ord. 4977 §8, 2006; Ord. 4950 §8 (Exh. A) (part), 2005)

CHAPTER 20.60 LOW DENSITY RESIDENTIAL (R-10)

20.60.040 CONDITIONAL USES.

The following uses may be permitted, provided a conditional use permit is approved pursuant to BMC 20.58.020 subject to the corresponding conditions:

- (a) Bed and breakfast, provided:
 - (1) The operators of the business shall occupy the house as their primary residence;
 - (2) No more than one (1) full-time equivalent (FTE) employee who is not a resident of the dwelling may be employed;
 - (3) No more than six (6) bedrooms are made available for rent to guests and all guest rooms are contained within the principal structure;

ATTACHMENT A.1

- ~~(4) Two (2) off-street parking spaces, plus one (1) off-street parking space per each guest bedroom, are required;~~
 - ~~(5) Off-street parking spaces may be reduced, provided the applicant can demonstrate parking will not spill over into nearby residential properties and any streets;~~
 - (46) Rooms shall not be made available to guests for more than fourteen (14) days during any thirty (30) day period;
 - ~~(57) No commercial receptions, parties, or other public gatherings, or serving of meals to nonresident guests for compensation, are allowed; and~~
 - ~~(68) Any remodeling of the residential structure shall maintain the residential nature of the structure and not alter the structure in such a manner that would prevent it from being used as a residence in the future.~~
- (b) Nursing/convalescent homes, provided:
- (1) All state licensing requirements are satisfied;
 - (2) Minimum site area shall be no less than one (1) acre;
 - (3) Minimum setbacks, height and lot coverage of the underlying zone shall apply;
 - ~~(4) Off-street parking shall be a minimum of one (1) space per six hundred (600) square feet of gross floor area;~~
 - ~~(5) The number of required off-street parking spaces may be reduced, provided the applicant can demonstrate that parking will not spill over into nearby residential properties and any streets; and~~
 - (46) Landscaping is provided meeting the minimum requirements for nonresidential uses prescribed in Chapter 20.50 BMC. Additional landscaping for screening purposes may be required if it is found necessary to mitigate any impacts to adjoining residential properties.
- (c) Day care facilities (thirteen (13) or more persons receiving care), provided:
- (1) All state licensing requirements are satisfied;
 - ~~(2) Off-street parking shall be a minimum one (1) space per each five (5) children based on the state license maximum occupancy load;~~
 - ~~(3) One (1) loading/unloading space without backup is required for the first twenty (20) children and one (1) additional space for up to each additional twenty (20) children;~~
 - ~~(4) The number of required off-street parking spaces may be reduced, provided the applicant can demonstrate that parking will not spill over into nearby residential properties and any streets;~~
 - (25) Landscaping is provided meeting the minimum requirements for nonresidential uses prescribed in Chapter 20.50 BMC. Additional landscaping for screening purposes may be required if it is found necessary to mitigate any impacts to adjoining residential properties; and
 - ~~(36) The maximum height of a fence or wall within a front yard setback may be increased up to six (6) feet, provided it enhances safety and security of an outdoor play area.~~
- (g) Schools, parks and associated uses may be approved in accordance with the following:
- (1) The following uses are permitted through approval of a conditional use permit:

ATTACHMENT A.1

- (i) All public schools and associated gymnasiums and auditoriums;
 - (ii) Private schools (K-12) with thirteen (13) or more students;
 - (iii) Parks and playgrounds greater than one-half (1/2) acre;
 - (iv) Outdoor athletic fields;
 - (v) Boat launching and related facilities;
 - (vi) Maintenance and service yards;
 - (vii) Bus and other vehicle and equipment maintenance and storage facilities;
 - (viii) Administrative office related to the facilities greater than two thousand (2,000) square feet gross floor area;
 - (ix) Buildings and structures for nonprofit groups on public lands;
- (2) Uses permitted pursuant to subsection (gj)(1) of this section shall be subject to complying with the following conditions:

CHAPTER 20.70 DISTRICT CENTER CORE (DCC)

20.70.060 DEVELOPMENT STANDARDS.

(e) Maximum building coverage: sixty (60) percent base maximum; provided, that through the use of any combination of the following, building coverage may be increased up to eighty-five (85) percent maximum:

- (5) Projects providing at least fifty (50) percent of their required parking underground or within the building: twenty (20) percent bonus.

20.70.080 PARKING REQUIREMENTS.

Parking shall meet the standards of Chapter 20.48 BMC and the following requirements: ~~For legally established existing buildings that comply with BMC 20.48.020(b) the parking will be exempt from the following requirements:~~

~~(a) For nonresidential uses, the minimum parking requirement is one (1) space per employee.~~

~~(b) On-site parking for nonresidential uses shall not exceed four (4) spaces per one thousand (1,000) square feet of floor area.~~

~~(a) On-site parking shall be to the rear or side of buildings and shall not occupy more than fifty (50) percent of the site frontage facing the arterial street(s). The site frontage includes the area between the right-of-way and front building wall which applies to the entire length of the property regardless of building width. Corner lots have two (2) site frontages as they are positioned on two (2) street frontages.~~

~~(b) All efforts shall be taken to avoid placing parking on street corners. Parking located between the building frontage and street corners shall be fully screened as follows:~~

- ~~(1) A four (4) foot tall decorative wall within the front yard landscaping area that fully screens the parking areas. The wall shall be located such that it blocks views of the parking from the right-of-way. For long spans of frontage (one hundred (100) feet or more), the wall shall include modular articulation to add architectural variety.~~

- ~~(i) Alternate architectural features or shrubs may be substituted for the wall, provided it is demonstrated that the shrubs/alternative will provide equal to or better visual screening than the wall. Shrubs shall be a minimum of three (3) feet tall at~~

time of installation and shall be additional to the landscaping required in Chapter 20.50 BMC.

(ii) Openings and architectural features may be required within a wall section in order to tie the wall feature into the architecture of the building and to provide pedestrian access. The entry shall be the minimum necessary to accommodate a sidewalk that is a minimum of five (5) feet in width, clearly marked, and distinguished from driving surfaces by using decorative paving, stamped/stained concrete, or raised walkways with alternative materials (such as brick, cobblestone, decorative pavers). Paint striping does not meet this requirement.

(ce) Access to parking shall be provided per the following priority:

- (1) First, from an alley if available; if an alley is not available, then from local street;
- (2) If neither is available, then from a nonprincipal arterial street;
- (3) If none of the above are available then from a principal arterial street.

(df) Driveways providing access to parking shall be well marked.

~~(g) Existing nonresidential structures adding no more than ten (10) residential units, and/or existing nonresidential structures expanding up to twenty-five (25) percent of the gross square footage of the building for any permitted use, shall be exempt from providing additional off-street parking, and exempt from substantial destruction requirements per BMC 20.54.070, provided:~~

- ~~(1) Projects shall be located within the Charleston district center;~~
- ~~(2) Mixed-use requirements per BMC 20.70.060(h) are maintained;~~
- ~~(3) No existing on-site parking or required landscaping is removed.~~

CHAPTER 20.78 MEDIUM DENSITY RESIDENTIAL (R-18)

20.78.040 CONDITIONAL USES.

The following uses may be permitted, provided a conditional use permit is approved pursuant to BMC 20.58.020 subject to the corresponding conditions:

(a) Bed and breakfast, provided:

- (1) The operators of the business shall occupy the house as their primary residence;
- (2) No more than one (1) full-time equivalent (FTE) employee who is not a resident of the dwelling may be employed;
- (3) No more than six (6) bedrooms are made available for rent to guests and all guest rooms are contained within the principal structure;
- ~~(4) Two (2) off-street parking spaces, plus one (1) off-street parking space per each guest bedroom, are required;~~
- ~~(5) Off-street parking spaces may be reduced, provided the applicant can demonstrate parking will not spill over into nearby residential properties and any streets;~~
- (46) Rooms shall not be made available to guests for more than fourteen (14) days during any thirty (30) day period;
- ~~(57) No commercial receptions, parties, or other public gatherings, or serving of meals to nonresident guests for compensation, are allowed; and~~

ATTACHMENT A.1

~~(68)~~ Any remodeling of the residential structure shall maintain the residential nature of the structure and not alter the structure in such a manner that would prevent it from being used as a residence in the future.

(b) Nursing/convalescent homes, provided:

(1) All state licensing requirements are satisfied;

(2) Minimum site area shall be no less than one (1) acre;

(3) Minimum setbacks, height and lot coverage of the underlying zone shall apply;

~~(4) Off-street parking shall be a minimum of one (1) space per six hundred (600) square feet of gross floor area;~~

~~(5) The number of required off-street parking spaces may be reduced, provided the applicant can demonstrate that parking will not spill over into nearby residential properties and any streets; and~~

~~(46)~~ Landscaping is provided meeting the minimum requirements for nonresidential uses prescribed in Chapter 20.50 BMC. Additional landscaping for screening purposes may be required if it is found necessary to mitigate any impacts to adjoining residential properties.

(c) Day care facilities (thirteen (13) or more persons receiving care), provided:

(1) All state licensing requirements are satisfied;

~~(2) Off-street parking shall be a minimum one (1) space per each five (5) children based on the state license maximum occupancy load;~~

~~(3) One (1) loading/unloading space without backup is required for the first twenty (20) children and one (1) additional space for up to each additional twenty (20) children;~~

~~(4) The number of required off-street parking spaces may be reduced, provided the applicant can demonstrate that parking will not spill over into nearby residential properties and any streets;~~

~~(25)~~ Landscaping is provided meeting the minimum requirements for nonresidential uses prescribed in Chapter 20.50 BMC. Additional landscaping for screening purposes may be required if it is found necessary to mitigate any impacts to adjoining residential properties; and

~~(36)~~ The maximum height of a fence or wall within a front yard setback may be increased up to six (6) feet, provided it enhances safety and security of an outdoor play area.

(g) Schools, parks and associated uses may be approved in accordance with the following:

(1) The following uses are permitted through approval of a conditional use permit:

(i) All public schools and associated gymnasiums and auditoriums;

(ii) Private schools (K-12) with thirteen (13) or more students;

(iii) Parks and playgrounds greater than one-half (1/2) acre;

(iv) Outdoor athletic fields;

(v) Boat launching and related facilities;

(vi) Maintenance and service yards;

(vii) Bus and other vehicle and equipment maintenance and storage facilities;

(viii) Administrative office related to the facilities greater than two thousand (2,000) square feet gross floor area;

- (ix) Buildings and structures for nonprofit groups on public lands;

CHAPTER 20.79 HIGH DENSITY RESIDENTIAL (R-40)

20.79.080 PARKING REQUIREMENTS.

Off-street parking shall be provided in accordance with the requirements set forth in Chapter 20.48 BMC. ~~In addition to the off-street parking requirements set forth in Chapter 20.48 BMC, the automobile off-street parking spaces located within the downtown regional center shall be provided in accordance to the Downtown Subarea Plan.~~

CHAPTER 20.82 NEIGHBORHOOD BUSINESS (NB)

20.82.060 DEVELOPMENT STANDARDS.

(e) Maximum building coverage: sixty (60) percent base maximum; provided, that through the use of any combination of the following, building coverage may be increased up to eighty-five (85) percent maximum:

- (5) Projects providing at least fifty (50) percent of ~~their required~~ parking underground or within the building: twenty (20) percent bonus;

CHAPTER 20.96 CITY UTILITY LANDS (CUL)

20.96.080 PARKING REQUIREMENTS.

Parking shall meet the standards of Chapter 20.48 BMC, ~~and the following requirements:~~

- ~~(a) Parking may not exceed two (2) stalls per residential dwelling unit;~~
- ~~(b) On-site parking requirements for a permitted or a conditional use may be reduced by up to forty (40) percent, provided it can be demonstrated through a parking analysis that the reduction would decrease impervious surface area, and the reduction would not cause parking to spill over into adjacent areas.~~

CHAPTER 20.97 WATERSHED (WS)

20.97.080 PARKING REQUIREMENTS.

Parking shall meet the standards of Chapter 20.48 BMC, ~~except required parking may be reduced or waived by the Director in the watershed (WS) zone in order to reduce impervious surface area.~~

ATTACHMENT A.2

2. Primary entrances shall be located so that they are visible and prominent from streets or access roads and parking areas.
 - i. Primary entrances shall be made visible and prominent by using architectural elements such as canopies, fixed seating, large doors, or protruding or recessed entrances.
 - ii. Avoid hidden building entrances, ensure good sight lines and well-lit inset doorway and alcoves to increase personal safety.
3. The primary public entrances of all buildings shall be enhanced by one or more of the following at or near the entry:
 - i. Provide pedestrian facilities such as benches, kiosks, special paving; or
 - ii. Provide a trellis, arbor or other building element that incorporates landscaping; or
 - iii. Provide pedestrian-scaled lighting; or
 - iv. Provide adjacent window displays; or
 - v. Provide building ornamentation such as mosaic tile, relief sculpture, ornamental wood, metal trim, or other approved detailing; or
 - vi. Provide artwork or special pedestrian-scaled signs; or
 - vii. Other methods as approved by the Director as meeting the intent.
- b. Basic Building Design Standards. One of the following basic building design standards are required for building façades longer than 100 feet that are visible from the public right-of-way:
 1. A visual treatment applied for a continuous distance of at least 20 feet along the façade with the interval between treated areas not exceeding 100 feet;
 2. An offset with a minimum depth of 5 feet;
 3. A façade material, texture or color that is visually different and distinct from that of the base material, texture or color;
 4. Landscape screening or other vegetated treatment as approved by the Director;
 5. This requirement may be waived or modified for heavy industrial uses where determined not to be feasible by the Director.
- c. Minimum Building Energy Efficiency. For all new construction, development shall meet at least one of the requirements, as described in LEED-ND GIB Prerequisite 2: Minimum Building Energy Efficiency, or functional equivalent standard as determined by the Director.

4.040 Transportation, Parking, Circulation, and Pedestrian Access

- a. The purpose of this section is to reduce the visual impact of parking, reduce vehicle trips, encourage alternate modes of transportation, and reduce greenhouse gas emissions within PSIC-B. Parking is regulated through standards that address the design, location, and size of parking areas. Right-of-way standards and requirements for sidewalks, trails, and driveways are contained in Chapter C.5. Traffic requirements and parking development shall be in accordance with the provisions of the Bremerton Municipal Code, including Title 10 (Traffic), Chapter 11.12 (Transportation Development), Chapter 20.48 (Off-Street Parking), with the following revisions and exceptions.
- b. Commute Trip Reduction
 1. The requirements of BMC Chapter 10.40, Commute Trip Reduction (CTR) Plan, shall apply, except as provided in paragraph (2) below.
 2. Once total new employment within PSIC-B has exceeded 2,000 new employees (resulting from actions permitted under the Planned Action Ordinance), all employers with 50 or more employees shall be required to participate in the CTR Program.
- c. Minimum Parking Requirements. Development is exempt from providing automobile off-street parking spaces, except as otherwise required for conformance with the Federal Americans with Disabilities Act (ADA) and the State of Washington.

d. Shared Parking Area Incentives

1. To qualify for incentives listed per Section 5.040, Exhibit PSIC-33, off-street parking for multiple buildings/tenants may share a common, centrally located parking area.

e. Parking Stall and Aisle Design. Shall be in accordance with BMC 20.48.080.

f. Loading and Vehicle Storage Areas

1. Loading and vehicle storage areas shall not be located within required building setbacks.
2. For sites fronting on Highway 3 or Lake Flora Road, vehicle loading docks and long-term vehicle storage areas shall be screened from public rights-of-way with Type I Visual Screening, walls, or other methods, as approved by the Director.
3. The Director may waive or modify these standards where it is deemed infeasible for a particular industrial or manufacturing use.

g. Pedestrian Access

1. Projects shall include an on-site pedestrian system connecting all parking areas and entrances to each other and the public right-of-way.
2. Pedestrian paths shall be integrated with the parking lot landscaping required in BMC 20.48.080 and BMC 20.50.050(c)(3).
3. Bicycle circulation shall be considered in site design and appropriate measures taken to accommodate bicycle circulation on-site.
4. Where feasible, provide steps and ramps across retaining walls and slopes.
5. Pedestrian paths shall be well lit.
6. Adjacent landscaping shall not block visibility to and from a pedestrian path, especially where it approaches a roadway or driveway.
7. Refer to Section 5.3 for more specific Design Guidelines.

h. Bicycle Facilities. Bicycle parking shall be provided consistent with BMC 20.48. Please see Section 5.3.170 for recommended Design Guidelines.

i. Neighborhood Electric Vehicles. Neighborhood Electric Vehicles (NEV) and Electric Golf Carts shall be allowed on all pedestrian pathways within PSIC-B that are constructed to the standards contained in Section 4.5.050. NEVs shall also be allowed on all public roadways in PSIC-B where their use is not otherwise prohibited by state or local law (e.g. roadways with a maximum speed limit of 25 miles per hour or less are suitable).

4.050 Landscaping

a. The purpose of this section is to ensure that site landscaping within PSIC reflects key goals related to sustainable development and stewardship of critical areas. The retention of existing trees and vegetation is important to help promote the utilization of natural systems for environmental benefits, reduce the impacts of development on the storm drainage system, moderate the urban heat island effect and provide a better transition between various land uses within the City. The requirements of BMC Chapter 20.50 shall apply, except as modified or supplemented in this subsection. Please also see Section 5.4 for recommended design guidelines.

b. Vegetation Preservation

1. To the greatest extent feasible, existing healthy significant trees defined in BMC 20.50.050(d)(4) shall be retained on site.
2. Site design shall attempt to preserve existing trees where feasible. However, should a proposal include the removal of all or portions of a site's significant tree cover a certified arborist or professional forester shall be retained by the applicant to inventory the tree(s) and make recommendations regarding the protection, retention, preservation, removal and replacement of the tree(s). A copy of the report and recommendations shall be submitted to the City as part of the site development process and prior to clearing.

Exhibit PSIC-33: Site Development and Building Design Measures and Points		
Standard	Description	Points
Access to Open Space	Project sets aside open space equal to at least 2% of the gross floor area of all structures for use by employees and visitors.	5 points
Connections to Existing Road Infrastructure	Site design for new development is configured in such a way as to allow future businesses and site occupants shared access to roads within or contiguous to the development site.	5 points
Supports and Serves Local Business	Land use is manufacturing, storage, or support retail and service uses that primarily serve customers located within Kitsap or Mason Counties or are directly related to the Puget Sound Naval Shipyard, Port of Bremerton operations, Naval Base Kitsap, or any other business that is already located within Kitsap or Mason Counties. NOTE: The applicant must provide sales or ownership documentation to receive 10 points.	5 Points. 10 points if the business is directly related to (subsidiary of or more than 50% of gross sales to) an existing business within the Bremerton City Limits.
Support Retail and Service Uses	Allow space for support retail and service uses in development clusters with more than 100,000 sq. ft. of floor space that consist of at least 60% industrial uses. The total square footage of support retail and services shall not exceed 20,000 sq. ft. or 10% of the total development cluster building space, whichever is less.	5 Points for one local service or retail use. 10 Points for two or more.
Shared access	Shared access driveway is provided and designed to serve two or more development sites (one may be a future site), a joint tenant building is provided on a site, or the project is located within a multi-tenant industrial park.	5 Points
Shared Parking	Shared parking is provided that serves two or more tenants. No additional parking outside of the shared lot(s) may be provided. Shared parking lots shall be located within a 1,200 foot radius of the front door of the building.	5 points
Shared Loading/ Service Court	Shared or consolidated loading areas are provided in a central service court or other location that is screened from public view.	5 Points
Job Density	Minimum of 10 jobs per acre employment density.	10 points
Innovative Measures	Points shall be awarded on a case-by-case basis, upon approval of the Director, to sustainable measures that are proven to promote compact, efficient development that maximizes the return on infrastructure investment and reduces VMT.	5 points per measure; no limit on the number of measures awarded points in this category

5.050 Sustainable Transportation Measures

- a. The purpose of this section is to provide measures that promote efficient multi-modal connections to services for employees, clients and other users while promoting increased use of transit, reduced vehicle trips, and reduction of greenhouse gas (GHG) emissions.

Exhibit PSIC-34: Sustainable Transportation Measures and Points		
Standard	Description	Points
On-Site Trail Construction	Pursuant to Section 4.5.050 or as proposed by the developer and agreed to by the City, dedication and construction of an off-street trail is provided.	5 points
Off-Site Trail Connections	Project provides a connection to an existing or future multi-modal trail system that connects site with at least one other service use or employment use.	5 points
Local Shuttle Service	Employer provides a free shuttle service that provides access to multiple work sites, services used by employees, park and ride lots and/or transit stops.	Minimum of 5 points. Up to 10 points depending

ATTACHMENT A.3

regulations.

LU1-Cen(F): Implement parking standards that prioritize Active Transportation, public transit, housing and community uses, rather than expansion of an automobile-based built environment.

LU2-Cen(A): Pre-qualify key areas and sites for environmental permitting through such tools as subarea plans and related programmatic Environmental Impact Statement's. Work toward enabling development in Centers to proceed as a Planned Action under the State Environmental Protection Act (SEPA) including coordination with the local tribal government for protection of treaty cultural and natural resources.

LU2-Cen(B): Coordinate with Kitsap Transit to provide transit access to centers.

LU2-Cen(C): Provide incentives and flexibility that encourage and enable development in Centers, including alternative parking options like payment in lieu of parking spaces.

LU3-Cen(A): Provide recreation opportunities within centers including access to the shoreline.

LU4-Cen(A): Improve and provide for walkability, and other nonmotorized transportation routes throughout Centers and provide links between the centers and neighborhoods.

The Comprehensive Plan also includes a policy specific to the Eastside Employment Center (now called Harrison Heights)

LU2-EC(A): Provide flexibility in the setback, height, density, building footprint, and lot area development regulations to encourage redevelopment of this area and promote use of Low Impact Development (LID) techniques and Best Management Practices (BMPs).

5.3.030 Multi-Use (MU) zone

5.3.030(A) Intent

The intent of the Multi-Use zone is to allow a range of commercial, office, residential and retail uses with improved non- motorized connections and amenities. In this zone, allowed uses and standards provide sites with maximum development flexibility to be single-purpose employment uses, residential uses, or uses mixed in a horizontal or vertical format. The development of building types will have a clear relationship to the street to promote activity, community-wide safety, and livability. Visual prominence of surface parking or garages are contrary to the pedestrian oriented housing characteristic of this zone.

5.3.030(B) Use Standards

- 1) Outright permitted uses pursuant to BMC 20.70.020, with the following additions:
 - (i) Light Industrial/Manufacturing
- 2) The following uses may be permitted, provided a conditional use permit is approved pursuant to BMC 20.58.020:
 - (i) Group residential facility - Class I
- 3) The following uses are prohibited in the Multi-Use Zone:
 - (i) Heavy Industrial/Manufacturing;
 - (ii) Outdoor storage, either as a primary use or accessory use;
 - (iii) Warehousing/Mini-storage
 - (iv) Single-family residential;
 - (v) Group Residential Facility - Class II.

5.3.030(C) Development Standards

Lot development shall be in accordance with BMC 20.70.060, with the following exceptions:

- 1) Minimum Floor Area Ratio (FAR) of .45;
- 2) Maximum Building Height. Eighty (80) feet for residential uses and sixty (60) feet for nonresidential uses. For mixed uses, the building height will be based on the use that occupies the majority of the structure (not including structured parking).
- 3) Minimum Density. The minimum required density is fifteen (15) dwellings per acre
- 4) Common and private open space per Harrison Heights Subarea Plan section 5.3.040;
- 5) Crime Prevention through Environmental Design section 5.3.050.
- 6) Mixed-use requirements per BMC 20.70.060(h) & BMC 20.70.070(a)(6) shall only apply within the boundaries of the Multi-Use Commercial Core Overlay;
- 7) Minimum twelve (12) feet floor to ceiling height at ground floor shall apply within the boundaries of the Multi-Use Commercial Core Overlay;

5.3.030(D) Design Standards

Design standards shall be applied to all new structures and redeveloped structures in accordance with BMC 20.70.070. Additionally, new structures and expansions of existing structures shall demonstrate considerations taken to orient the project toward the Bridge-to-Bridge trail, waterfront, or other public gathering space recognized by the Director. This may be accomplished through architecture, site design, public art, or other features accepted by the Director.

5.3.030(E) Parking Requirements, pursuant to BMC 20.70.080.

5.3.030(F) Landscape Requirements pursuant to BMC 20.50

5.3.030(G) Sign Standards pursuant to BMC 20.52

ATTACHMENT A.4

IV - ZONING & DEVELOPMENT REGULATIONS

A. LAND USE ZONES

This section of the Sub-Area Plan contains the zoning and development standards that will guide the redevelopment of Bay Vista and will implement the requirements of the site's zoning designation. The standards address the following elements:

Land uses/zones, including density, height, setbacks, building coverage and impervious surface coverage;

- General development standards;
- Building design;
- Streets, sidewalks and driveways;
- Parking;
- Landscaping;
- Parks and open space;
- Signs.

Design standards are reviewed for conformity by the ARC, as discussed in Sec. III.

The Bay Vista site was developed as a Master Development Plan (MDP) per the 2004 Comprehensive Plan. These proposals must meet a public need and provide a community benefit, such as redeveloping “blighted” areas or providing affordable housing opportunities. Projects may be proposed by public entities or public-private partnerships, and should result in innovative development.

The Bay Vista development was required to prepare a Master Development Plan (MDP), which includes standards for development of the site, including uses, density, setbacks, lot coverage and heights. Other required components of an MDP, which are addressed in these regulations, include the following:

- An open space plan that protects natural resources and provides recreational opportunities;
- Site edge design that is compatible with adjacent uses and densities;

- Streetscapes that include amenities, (street trees, planting strips, benches, street lighting, etc.) and other design features that make streets interactive, walkable and enjoyable;
- A general landscape plan, including preservation of significant trees where possible;

1. LAND USE & DIMENSIONAL STANDARDS

Land uses within Bay Vista and applicable dimensional standards are identified in **Table A-1**, Density and Dimensional Standards and **Figure A-2**, Land Use Map.

2. RESIDENTIAL

Intent: The intent of the residential (R) zone is to accommodate a variety of housing types at varying densities consistent with creation of a mixed-income, mixed-use pedestrian-oriented community. Housing types may include single-family attached and detached in a variety of styles. Multi-Family housing may also occur in mixed-use buildings.

Residential standards shall be in accordance with the standards of BMC 20.60 with the following additions and/or revisions:

- i. Permitted Uses and Density
 - a. Permitted Uses
 - (i) Provided that they are consistent with the intent of the Zone, all uses are permitted outright, provided that the Development Standards specified in Section IV are satisfied. The applicant shall bear the burden of proving that a proposed use achieves the stated intent of the particular zone.
 - b. Use Definitions
 - (i) Definitions of the specific land uses are found in BMC Chapter 20.42.
 - c. Decision Authority
 - (i) A use determination made by the Director may be appealed to the Hearing Examiner following a Type II decision process as set forth in BMC Chapters 20.02 and 20.40

- d. Compact spaces shall be a minimum of 15 feet in length. Standard spaces shall be a minimum of 18.0 feet in length; up to 2.0 feet may overhang curb or wheel stops.
- e. Exceptions to the above minimum dimensions shall be granted for handicapped accessible spaces. Applicable state and federal codes and requirements for accessible parking spaces shall supersede any requirements herein.
- vi. Parking and loading areas shall be graded for proper drainage and provided with a suitable surface, as approved by the city engineer.
- vii. All driveways and areas used for loading, parking and maneuvering motorized vehicles shall be paved. Gravel surfaces are not allowed.
- viii. Each parking or loading space must be usable and readily accessible.
- ix. Required parking and loading areas shall be provided with designated entrances and exits located so as to minimize traffic congestion and avoid undue interference with public use of streets, alleys and walkways.
- x. Parking and loading areas provided in accordance with the requirements of these standards shall not be used for the sale, repair, assembly or disassembly, storage or servicing of vehicles or equipment. Unlicensed and inoperable vehicles shall not be stored in any required parking or loading space.
- xi. If existing parking lots are re-striped, the new layout of the parking spaces shall be the same as the previous layout; or if changed, the changed layout shall conform to the existing dimensional requirements of this chapter.

2. COMPACT CAR ALLOWANCE

- i. A maximum of 30 percent of the total required off-street parking stalls may be permitted and designated for compact cars in Village Commercial.
- ii. Each compact stall shall be designated as such.
- iii. Dimensions of compact parking stalls shall conform to the standards as depicted in this chapter.
- iv. The maximum compact car allowance may be exceeded if an applicant submits a vehicle composition study, relevant to the type of development, which demonstrates that a greater number of compact vehicles should be accommodated.

3. BICYCLE PARKING

- i. The required minimum number of bicycle parking spaces shall be consistent with BMC 20.48. Bicycle parking spaces are meant to accommodate residents, employees, visitors, customers, messengers, and other persons.
- ii. Bicycle parking shall be located on site within 50 feet of well-used entrances and not farther from the entrance than the closest motor vehicle parking space. Bicycle parking may be located within city right-of-way in areas without building setbacks, subject to approval of the director and provided it meets the other bicycle parking requirements.

4. DRIVE-THROUGH STACKING REQUIREMENTS

- i. A stacking space shall be an area measuring 8 feet by 20 feet with direct forward access to a service window of a drive-through facility. A stacking space shall be located to prevent any vehicles from extending onto the public right-of-way, or interfering with any pedestrian circulation, traffic maneuvering, or other required parking areas. Stacking space for drive-through or drive-in uses may not be counted as required off-street parking spaces.
- ii. For each drive-up lane a minimum of three stacking spaces shall be provided.
- iii. Stacking spaces serving businesses that are located adjacent to a less intensive zone than that in which the business is located, or are located adjacent to a public use area (such as a street, sidewalk, park, or trail) shall be landscaped to provide a buffer between the stacking spaces and the adjacent zone or public use.

5. STRUCTURED PARKING

- i. Structured parking shall be located behind the block perimeter buildings. Structured parking may also be located below buildings.
- ii. Structured parking shall be screened, except at the actual vehicular entry points and shall be screened.
- iii. Vehicular and service entries to garages shall be designed to look like a part of the building.
- iv. Parking garages: Parking garages with flat roofs may not exceed the height of the shortest building on the block when a flat roof is present on any building on the block, and parking garages with pitched roofs may not exceed the height of the shortest building on the block by more than one story.

6. ON-STREET PARKING

- i. On-street parking shall count as visitor parking. Each 24 feet of curb length, excluding 5 feet on either side of a curb cut, shall count as one visitor parking space.

ATTACHMENT A.5



Development Standards

Introduction

The development standards will set the zoning regulations for the East Park development. This section will illustrate lot standards, approximate open space locations and road standards. The intention of the standards is to provide a framework that guides this development into a successful neighborhood that will provide a mixture of compatible housing types. It becomes essential for all design elements such as; uses, setbacks, height, bulk, open spaces, and circulation to coexist and complement each other.

Each sub area development zone; low, medium, high and mixed use will allow a variety of product types. The development standards illustrate multiple design examples within each zone. The variation within a zone is driven by topography, relationship to open space, emergency vehicle access, and building scale. In short, one lot standard for each zone is not enough to allow diversity and meet the Comprehensive Planning goals.

- The maximum area of all accessory structures exclude attached and detached garages.
- Maximum height of attached or detached carriage units shall not exceed the height limit of the zone.

Open Space

- Where use easements exist, required open space may include the area on the adjacent lot covered by the use easement.
- The open spaces shall be maintained by a homeowners association
- All landscape features and bioswales within the public ways shall be maintained by a homeowners association.
- Prior to construction, all significant trees within the 'open space' designation affected by construction activities shall be evaluated by a certified arborist. The arborist shall prepare a report and clearly outline necessary steps to ensure survival of healthy significant trees during and after construction.
- Wildlife corridors shall be planted and maintained appropriately for wildlife habitat in accordance with a wildlife biologist or the best available scientific evidence supporting the actions taken.

General Development Standards

Building Separation

Where building separation requirements exist, the separation shall be the average, however in no case shall it be less than the required side yards combined.

Accessory Dwelling Units (ADUs)

- Entrance may be on any facade including that of the principal structure.
- Not included in maximum density requirements.
- Carriage units maximum garage structure shall be limited to 1,100 sq ft when serving a remote residential unit.

Signage

Per section BMC 20.52 with the following exceptions:

- Mixed Use areas shall comply with Commercial District Requirements BMC 20.52.100
- All other areas shall comply with residential requirements BMC 20.52.120

Fencing

Per BMC 20.46.020 with the following exceptions:

- Cyclone fencing not allowed except for dog runs of a maximum of 72 sq ft



Development Standards

- and screened from adjoining property.
- Front yard fencing a maximum height of 42 inches with a minimum of 25% open.
- 6 ft. high fence allowed in secondary frontage with 50% open above 42 inches.

Accessory Structures

Per BMC 20.44.060

Trash and Recycling Collection

- Residential curb side collection for all attached and detached single family housing.
- If collection area is established it shall be screened from the public realm by a minimum 5 ft wide landscape buffer.

Mailboxes

- Individual “door slot” delivery is preferred, which is the common delivery method for Manette. If delivered mail box kiosks are used, they shall conform to architectural character of neighborhood structures.

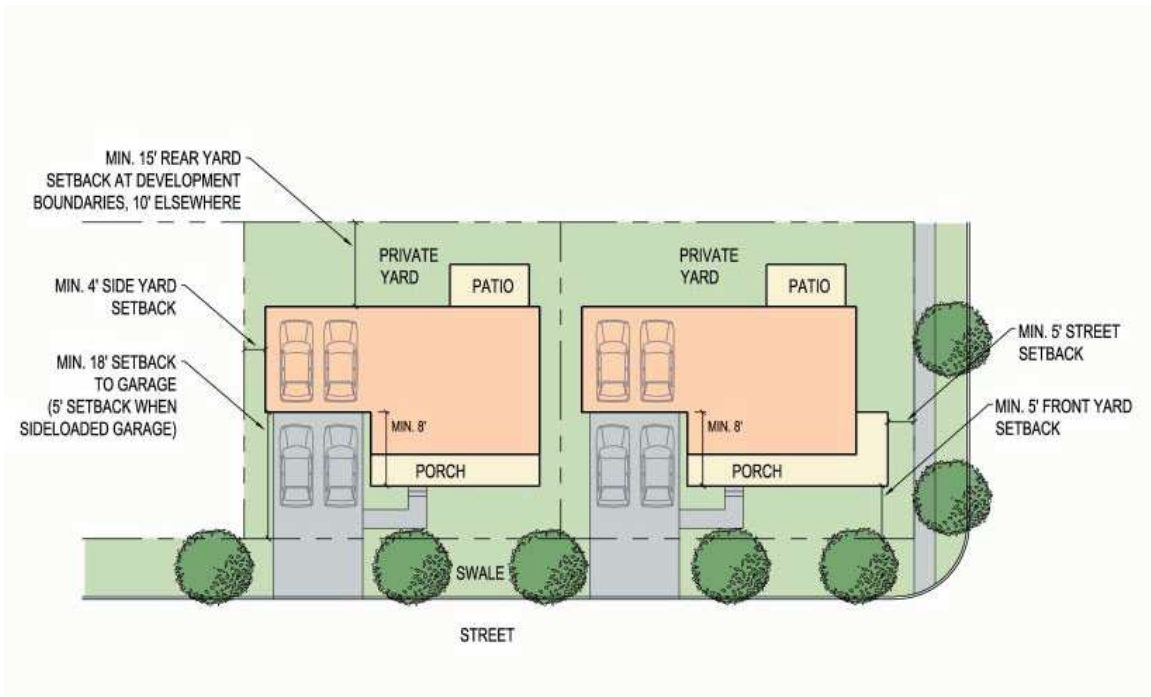
Fire Systems

- Some of the road width standards presented in the plan are reductions in that required by the International Fire Code and the BMC. All residential structures that are accessed from reduced width fire apparatus access roadways in the project area shall be equipped through out with a fire sprinkler system.



Low Density

Traditional Single Family Lot



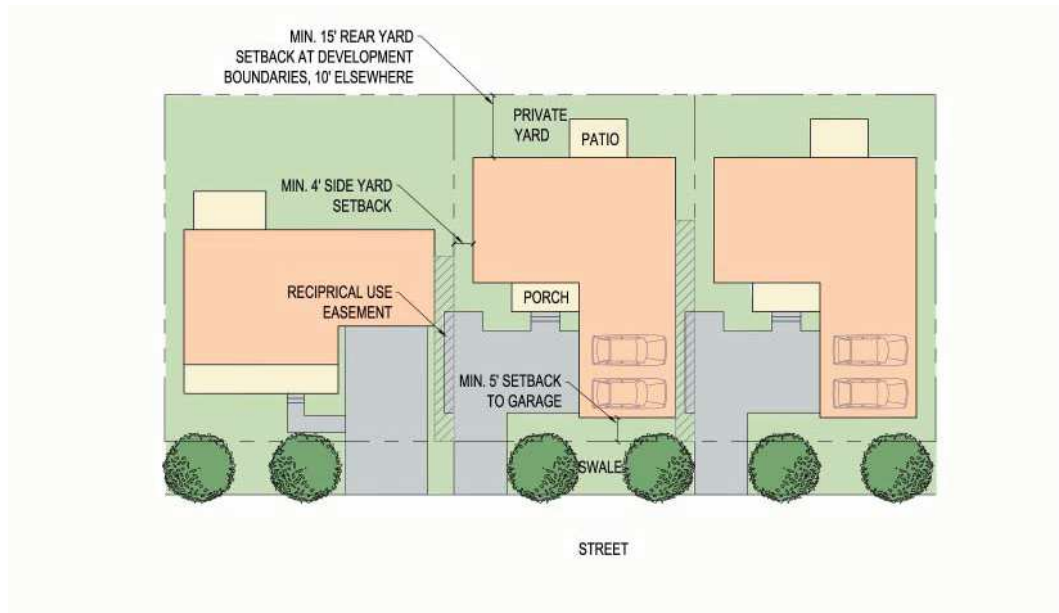
maximum building mass	lot requirements	additional standards
<p>building height - 35' measured from average finished grade to average roof height</p> <p>building coverage - 60%</p> <p>development coverage - 70%</p>	<p>minimum lot size - 3600 sq ft</p> <p>minimum lot width - 50'</p> <p>minimum lot depth - 60'</p> <p>street setback - 5' minimum</p> <p>side setback - 4' minimum</p> <p>rear setback - 10' minimum</p> <p>open space - 400 sq ft</p>	<ul style="list-style-type: none"> - Street facing garage limited to 50% of building width. - Minimum 18' setback to garage doors.

Development Standards



Low Density

Traditional Single Family Lot-Side Load Garage



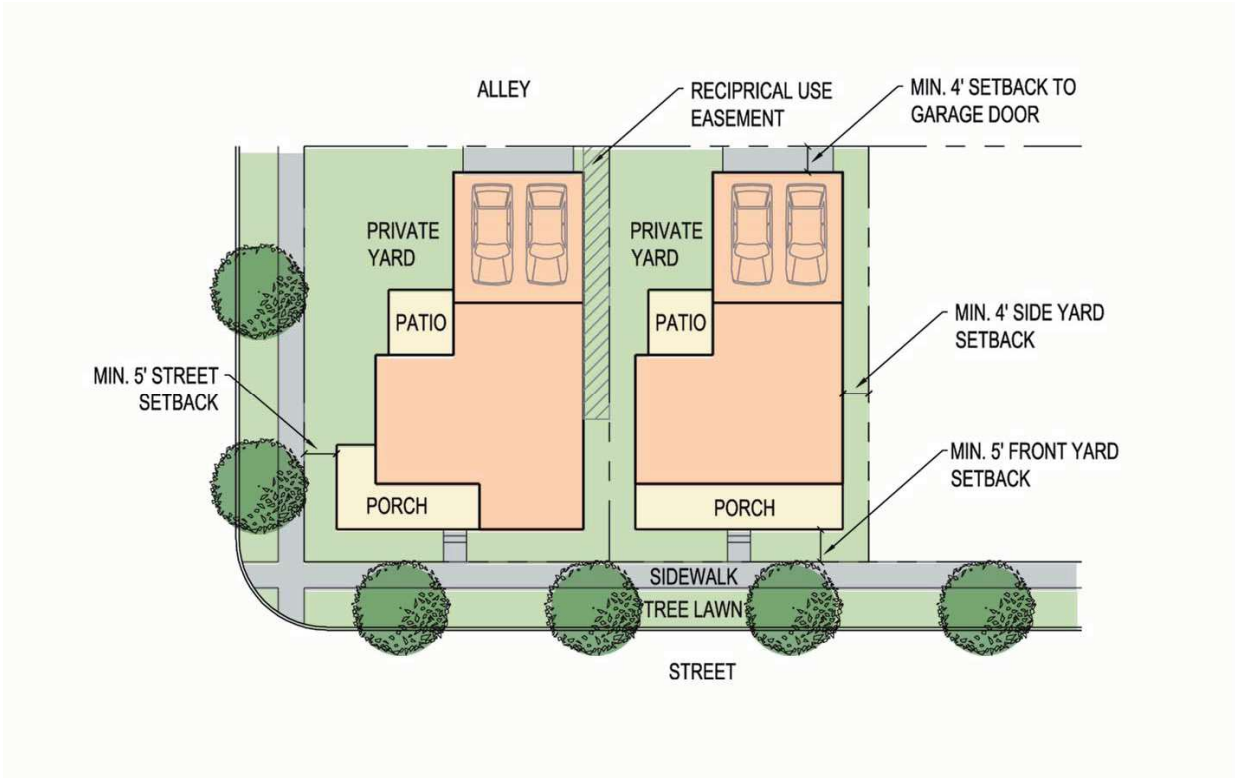
Development Standards

maximum building mass	lot requirements	additional standards
<p>building height - 35' measured from average finished grade to average roof height</p> <p>building coverage - 60%</p> <p>development coverage - 70%</p>	<p>minimum lot size - 3600 sqft</p> <p>minimum lot width - 50'</p> <p>minimum lot depth - 60'</p> <p>street setback - 5' minimum</p> <p>side setback - 4' minimum</p> <p>rear setback - 10' minimum</p> <p>open space - 400 sq ft</p>	<ul style="list-style-type: none"> - Street facing garage limited to 50% of building width. - Minimum 18' setback to garage doors.



Medium Density

Single Family Alley Lot



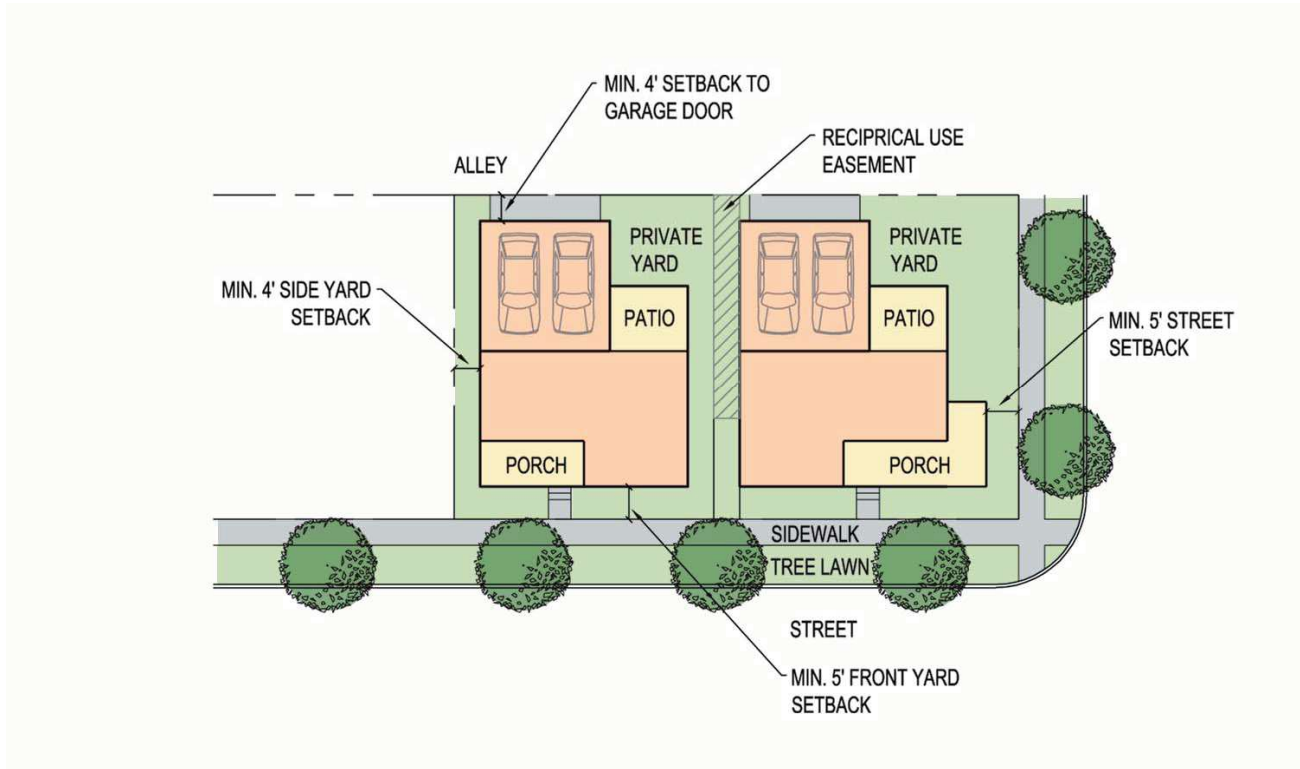
Development Standards

maximum building mass	lot requirements	additional standards
<p>building height - 35' measured from average finished grade to average roof height</p> <p>building coverage - 70%</p> <p>development coverage - 75 %</p>	<p>minimum lot size - 2000 sq ft</p> <p>minimum lot width - 34'</p> <p>minimum lot depth - 60' open space- 300 sq ft</p> <p>street setback- 5' minimum</p> <p>rear setback- 0' minimum at alley and 4' minimum to garage door</p> <p>side setback - 4' minimum</p>	



Medium Density

Single Family Lot with Basement Garage



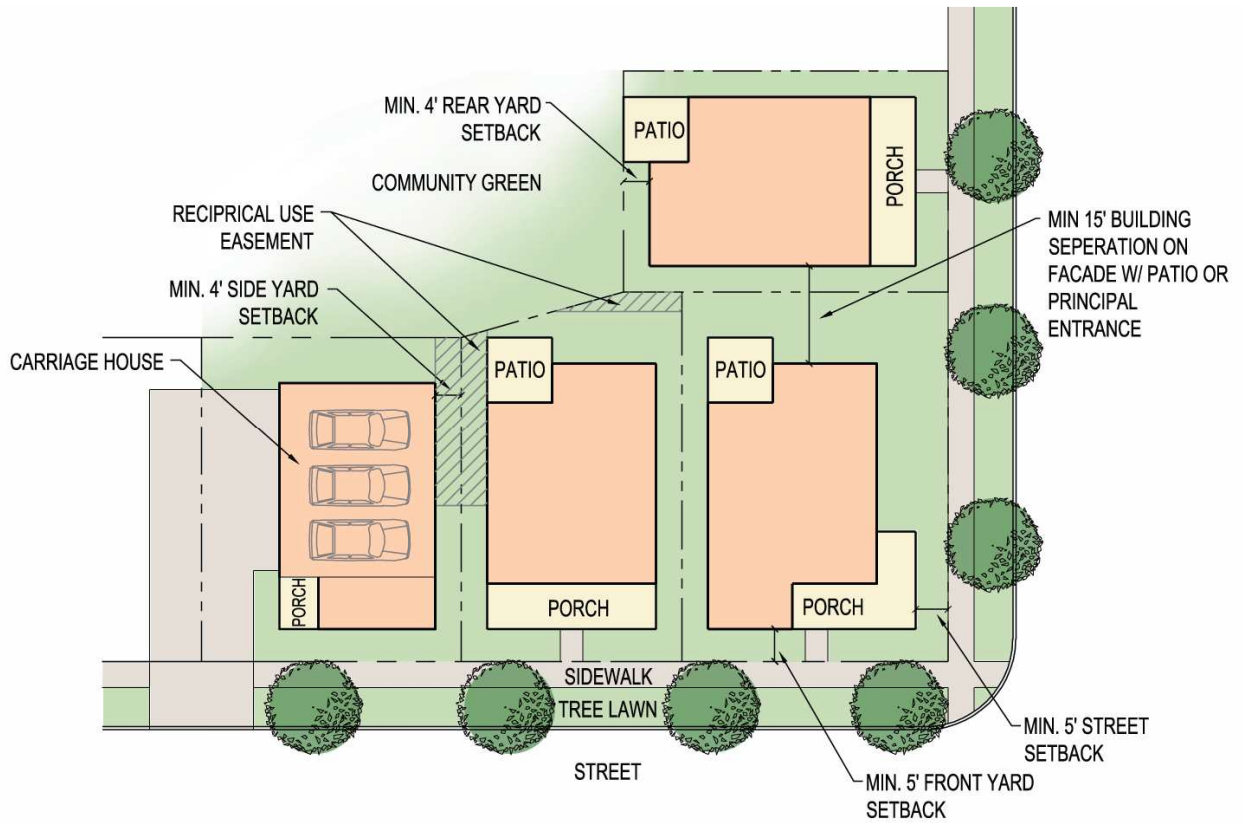
building mass	lot requirements	additional standards
<p>building height - 35' measured from average finished grade to average roof height</p> <p>building coverage - 70%</p> <p>development coverage - 75%</p>	<p>minimum lot size - 1700 sq ft</p> <p>minimum lot width - 34'</p> <p>minimum lot depth - 50'</p> <p>street setback - 5' minimum</p> <p>side setback - 4' minimum</p> <p>rear setback - 0' minimum at alley and 4' to garage door</p> <p>open space - 300 sq ft</p>	

Development Standards



Medium Density

Cluster Development Lot



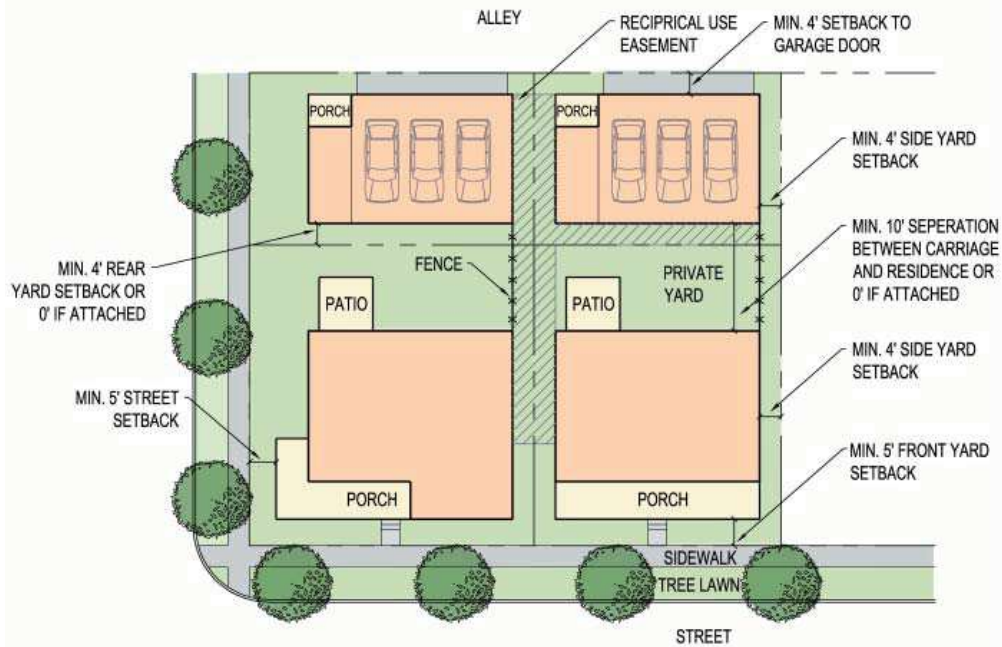
Development Standards

building mass	lot requirements	additional standards
<p>building height - 35' measured from average finished grade to average roof height</p> <p>building coverage - 70%</p> <p>development coverage - 75%</p>	<p>minimum lot size - 1700 sq ft</p> <p>minimum lot width - 34'</p> <p>minimum lot depth - 50'</p> <p>street setback - 5' minimum</p> <p>side setback - 4' minimum</p> <p>rear setback - 4' minimum</p> <p>open space - 200 sq ft on lot and or common green</p>	



High Density

Single Family Lot with Carriage Garage



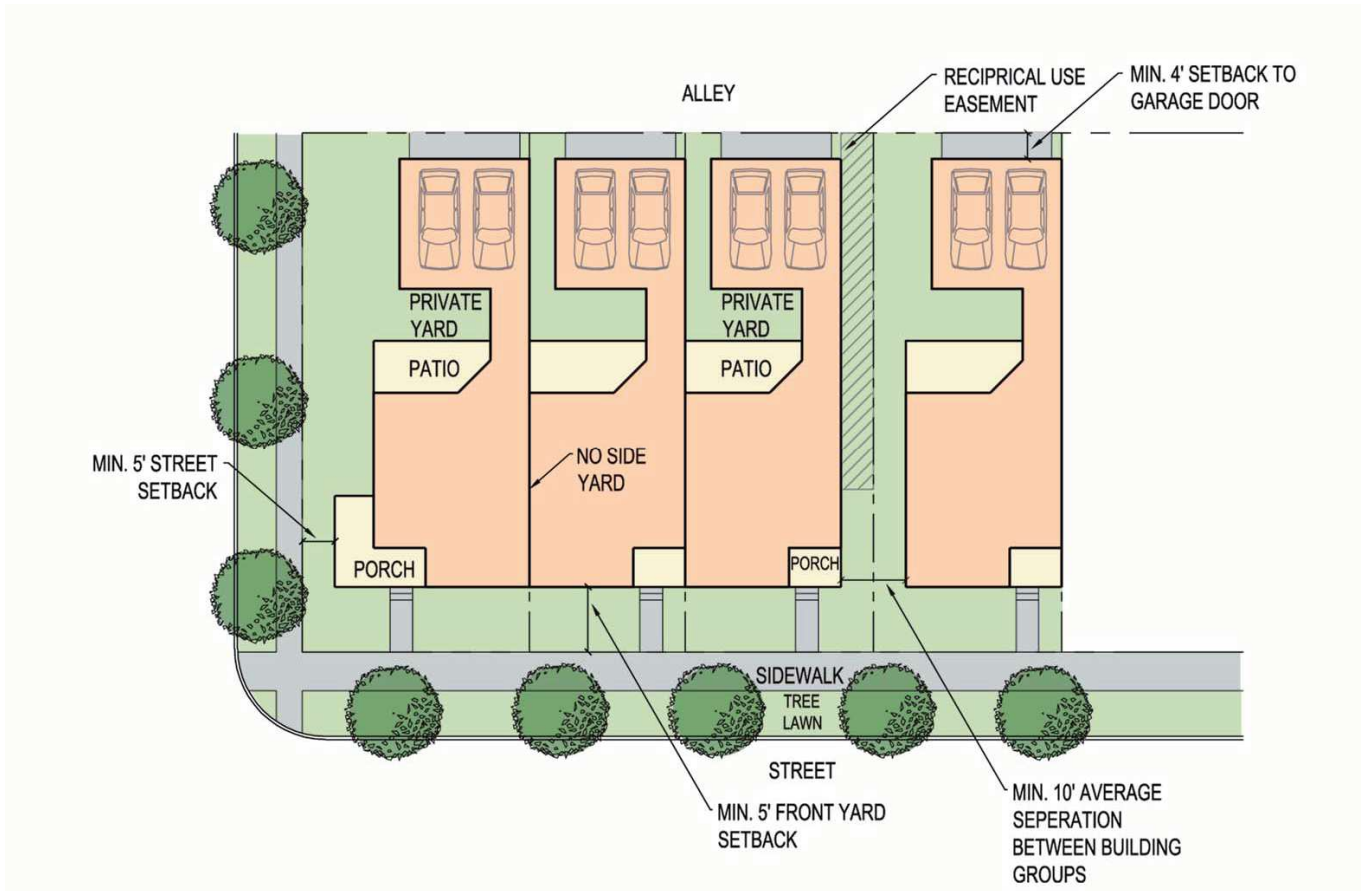
Development Standards

building mass	lot requirements	additional standards
<ul style="list-style-type: none"> <i>residential lot</i> building height - 35' measured from average finished grade to average roof height building coverage - 75% development coverage- 85% <i>carriage lot</i> building height - 35' measured from average finished grade to average roof height building coverage- 80% development coverage- 90% 	<ul style="list-style-type: none"> <i>residential lot</i> minimum lot size - 2,500 sq ft minimum lot width - 46' minimum lot depth - 56' street setback - 5' minimum side setback- 4' minimum rear setback- 0' minimum to alley and 4' to garage door open space - 300 sq ft <i>carriage lot:</i> minimum lot size - 850 sq ft minimum lot width - 32' minimum lot depth - 28' street setback - 5' min side setback - 4' minimum or 0' if attached rear setback - 0' minimum to alley and 4' to garage door open space - 0 sq ft 	



High Density

Townhome Lot



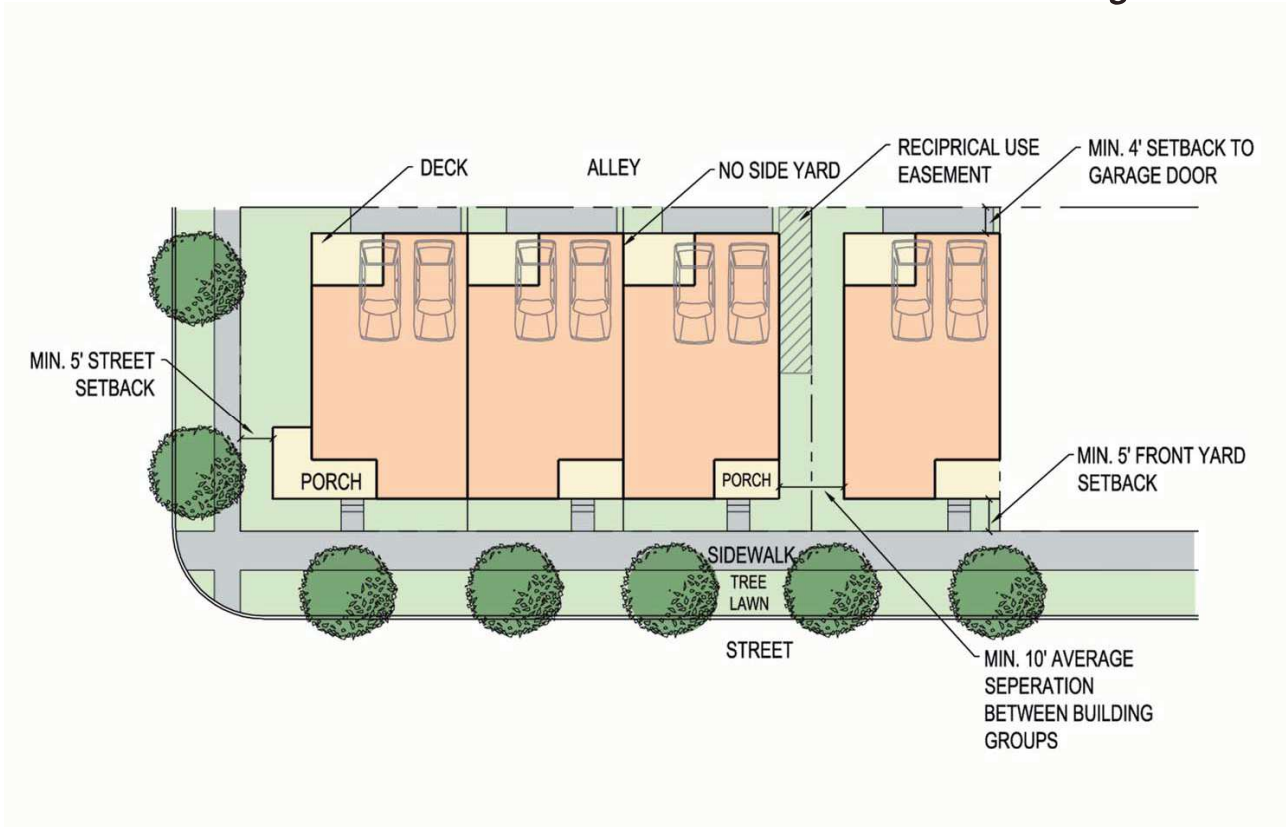
building mass	lot requirements	additional standards
<p>building height - 40' measured from average finished grade to average roof height</p> <p>building coverage - 80%</p> <p>development coverage - 90%</p>	<p>minimum lot size - 1500 sq ft</p> <p>minimum lot width - 20'</p> <p>minimum lot depth - 78'</p> <p>street setback- 5' minimum</p> <p>side setback- 4' minimum or attached</p> <p>rear setback- 0' minimum to alley and 4' to garage door</p> <p>open space - 200 sq ft</p>	

Development Standards



High Density

Townhome with Basement Garage Lot



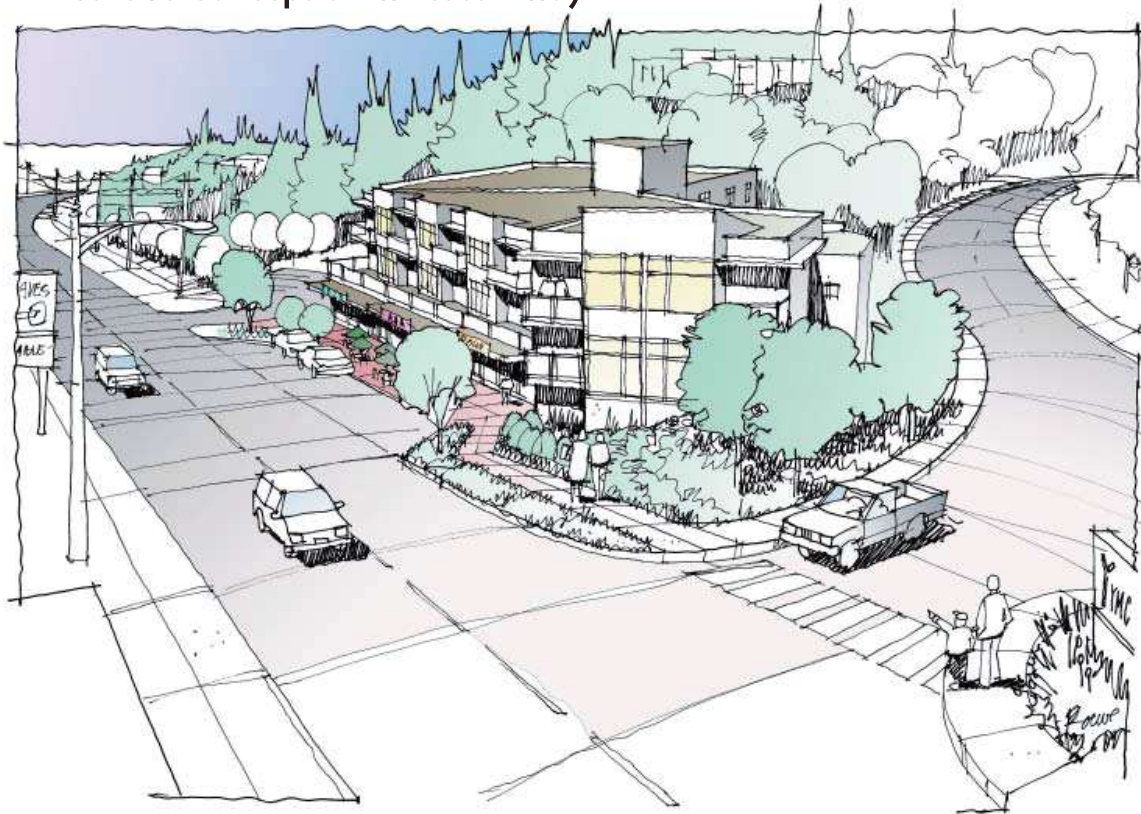
Development Standards

building mass	lot requirements	additional standards
<p>building height - 40' measured from average finished grade to average roof height building coverage - 95% development coverage - 100%</p>	<p>minimum lot size - 600 sq ft minimum lot width - 14' minimum lot depth - 44' street setback - 5' minimum side setback- 4' minimum or attached rear setback- 0' minimum to alley and 4' to garage door open space - 60 sq ft deck or porch</p>	



Mixed Use Density

Mixed Use Concept on Wheaton Way



Development Standards

building mass	lot requirements	additional standards
<p>building height - 60' measured from average finished grade to average roof height</p> <p>building coverage - 90%</p> <p>development coverage - 100%</p>	<p>street setback - 0' minimum</p> <p>other setback- 10' minimum</p> <p>open space- 10% of site usable</p>	<p>Retail, commercial or office space is allowed at ground level with residential uses above. Stand alone buildings of retail, commercial or office buildings are not permitted.</p>

DRAFT FINDINGS AND DETERMINATION OF THE CITY OF BREMERTON PLANNING COMMISSION

Summary – The Commission recommends that the City Council adopt the proposed amendments to Bremerton Municipal Code (BMC) Title 20, Puget Sound Industrial Center (Bremerton) Subarea Plan, Harrison Heights Subarea Plan, Bay Vista Subarea Plan, and East Park Subarea Plan. The proposed development regulations would further implement the vision of the 2024 Comprehensive Plan update and meet the statutory requirements of Washington State.

I. FINDINGS OF FACT - GENERAL

1. Project Description

Title 20 of the Bremerton Municipal Code (BMC), and individual subarea plans, regulate Land Use for the City, which includes the uses, development regulations, and permitting procedures related to development of individual lots. Zoning Code Amendments proposed with this report are intended to further support implementation of the 2024 Comprehensive Plan Update, and comply with several State mandates.

Substantive amendments proposed can be summarized by the following:

- Zoning Code (Title 20): amendments are proposed to BMC 20.12.175 Boundary Line Adjustment for code clarification, BMC 20.46.010 Accessory Dwelling Units for market-based parking, BMC 20.46.030 Home Occupations for market-based parking, BMC 20.46.070 Adaptive Reuse of Commercial Buildings for market-based parking, BMC 20.46.090 Temporary Encampment Permit for market-based parking, BMC 20.46.170 Cottage Housing for market-based parking, BMC 20.46.180 Group Residential Class I for market-based parking, BMC 20.46.190 Senior Housing Complex for market-based parking, BMC 20.46.200 Manufactured Home Park for market-based parking, BMC 20.46.220 Family Day Care Provider to comply with RCW 36.70A.450, BMC 20.46.220 Co-living Housing to comply with HB 1998, BMC 20.46.230 Neighborhood Scale Commercial to allow small-scale commercial uses in residential zones, BMC 20.48 Off-street Parking Requirements for market-based parking and comply with SB 6015, BMC 20.54.060 Nonconforming Uses for market-based parking, BMC 20.54.080 Nonconforming Landscaping for market-based parking, BMC 20.54.120 New Housing in Nonconforming Buildings to conform with RCW 35.21.990, BMC 20.58.060 Residential Cluster Development for market-based parking, open space alterations, and revisions to lot requirements, Low Density Residential BMC 20.60.040 for market-based parking, BMC 20.70 District Center Core for market-based parking, Medium Density Residential BMC 20.78.040 for market-based parking, BMC 20.79 High Density Residential for market-based parking, BMC 20.82 Neighborhood Business for market-based parking, BMC 20.96 City Utility Land for market-based parking, and BMC 20.97 Watershed for market-based parking.

- Puget Sound Industrial Center-Bremerton (PSIC-B) to implement market-based parking.
- Harrison Heights Subarea Plan (HHSAP) to implement market-based parking.
- Bay Vista Subarea Plan (BVSAP) to implement market-based parking.
- East Park Subarea Plan (EPSAP) to implement market-based parking.

2. Procedural History

Proposed amendments were noted in the April 7, 2025 2024 Comprehensive Plan Update Final Environmental Impact Statement (FEIS). Notification of the FEIS, and availability of Final Draft Documents for the Comprehensive Plan, was sent at that time to agencies, affected tribes, and other interested parties. Feedback for proposed development regulations was sought concurrently with the comprehensive planning process:

- **2.1 Planning Commission Workshop: Draft Downtown Subarea Plan and Parking Standards – February 26, 2024**
- **2.2 Planning Commission Workshop: Draft Downtown Subarea Plan – July 15, 2024**
- **2.3 Planning Commission Workshop: Land Use and Housing Appendices, and Downtown Subarea Plan, Alternative Parking Standards – September 16, 2024**
- **2.4 Draft Environmental Impact Statement issued October 18, 2024** Public comment period through December 2, 2024
- **2.5 Planning Commission Workshop: Draft Environmental Impact Statement – October 21, 2024**
- **2.6 Planning Commission Workshop: Transportation Element/Draft Active Transportation Plan & Zoning Code Amendments – January 27, 2025**
- **2.7 Planning Commission Workshop: Off-Street Parking – February 24, 2025**
- **2.8 Planning Commission New Business items, April 21, 2025**
- **2.9 Department of Commerce review submittal, May 12, 2025**
- **2.10 Planning Commission Public Hearing: Development Regulations Recommendation – May 19, 2025.** Planning Commission made recommendation to City Council for adoption.

3. Public Comment

Public comment was received throughout the duration of the Comprehensive Plan Update. Comments were received in various formats including letters, e-mails, survey responses, postcard, and public testimony at workshops and hearings. The Planning Commission considered all public comment provided. The Department of Community Development has maintained a written comment matrix with Staff response throughout the project and has maintained an Interested Parties list that was used for public notification. See Public Participation Appendix, Comprehensive Plan Section 8.7, for more detail.

4. SEPA Determination

As the City is updating the current (2016) Comprehensive Plan and associated currently adopted development regulations, much of the environmental review will continue to be applicable, however additional review needed to be conducted to address the minor changes. The City provided a Determination of Significance and Scoping Notice for an Environmental Impact Statement (EIS) on December 28, 2022. The public comment period for scoping comments expired on January 27, 2023. A Draft EIS (DEIS) was issued October 18, 2024 with the public comment deadline of December 2, 2024. The Notice of Availability for the Final Environmental Impact Statement (FEIS) was issued on April 7, 2025.

5. Consistency

Text amendments to Title 20 shall meet the decision criteria outlined in BMC 20.18.020(d). The Planning Commission may recommend a decision, and the City Council may adopt or adopt with modifications, amendments to Title 20 if the following criteria are met:

5.1 The amendments are consistent with the goals and policies of the Comprehensive Plan:

Analysis: The proposed amendments continue to uphold the objectives and goals of the Comprehensive Plan, and implement the following policies:

- Housing Element, Policy H2(A).
Support the private sector's efforts to provide a full range of housing options to meet the needs of all ages and demographics.
- Housing Element, Policy H3(A).
Provide opportunities for the production of new housing for all incomes, ages, and family types through infill by stimulating growth of non-traditional housing types such as townhomes, carriage units, accessory dwelling units, and duplexes in locations where they will seamlessly infill into the fabric of the existing neighborhoods.
- Housing Element, Policy H4(F).
Promote increased housing density to provide a broader customer base for more affordable public services including utilities.
- Land Use Element, Policy LU1(B).
Coordinate Bremerton's growth consistent with the Kitsap Countywide Planning Policies and the Puget Sound Regional Council's Vision 2040, and state requirements.
- Land Use Element, Policy LU3(I).
Promote infill and concurrent infrastructure improvements in areas that are already developed in order to preserve rural areas, open spaces, and ecological functioning lands within the City.

III. CONCLUSIONS & RECOMMENDATION

The Planning Commission concludes that the proposed amendments to the Bremerton Municipal Code Title 20 Land Use Chapter meets the requirements in BMC 20.18.020(d) text amendments, and therefore recommends to the City Council the adoption of amendments to Title 20 and various Subarea Plans, following adoption of the 2024 Comprehensive Plan major update.

Respectfully submitted by:

Approved by:

Andrea L Spencer, AICP
Executive Secretary

Nick Wofford, Chair
Planning Commission



PLANNING COMMISSION NEW BUSINESS

Prepared by: Garrett Jackson, Planning Manager

Meeting Date: May 19th 2025 Meeting

The Planning Commission will continue to review potential Zoning Code Amendments in 2025 related to implementation of the 2024 Comprehensive Plan update. The following list provides the Commission with a preview of topics to be considered at later public workshops in 2025, and provides an opportunity for early feedback.

June 16, 2025 Workshop Items:

BMC 20.46, Special Development; Transitional Housing, Permanent Supportive Housing, Indoor Emergency Shelters, and Indoor Emergency Housing: Standards within BMC 20.46 regulate components of the zoning code that require additional consideration, beyond that found within individual zoning districts. The State of Washington requires Transitional Housing, Permanent Supportive Housing, Indoor Emergency Shelters, and Indoor Emergency Housing per RCW 35.21.683. Draft code for these items will be presented to the Commission at the June workshop.

PUBLIC COMMENT RECEIVED

Garrett Jackson

From: Jim McDonald <jimmc90@gmail.com>
Sent: Tuesday, May 13, 2025 12:32 PM
To: Garrett Jackson
Subject: Comments on Proposed Development Regulations for the Comp Plan

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Garrett,

Congrats for getting this far. What a ton of work!

I won't be able to attend the next Planning Commission meeting but was hoping you could pass on my comments to the Commissioners.

Generally, I am in favor of the proposed changes.

I enthusiastically support these specifically: (Though I do think the 3-year lookback on BMC 20.54 is arbitrary and too restrictive.)

BMC 20.46 Neighborhood scale commercial

BMC 20.54 New Housing in Nonconforming buildings

BMC 20.70 Market Based Parking Standards

G

If the Planning Commission would apply more "Market Based" standards in other areas where certain businesses are restricted (e.g., storage units) then that would be an additional improvement. Focus more on the design standards. The City does such a good job in that area.

R, Jim McDonald Bremerton, WA 98310

PUBLIC COMMENT RECEIVED

Garrett Jackson

From: Travis Merrigan <bikebremerton@gmail.com>
Sent: Friday, May 16, 2025 2:59 PM
To: Garrett Jackson
Subject: Fwd: Bremerton Ponders Ending Parking Mandates to Spur Housing - The Urbanist

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Garrett,

Would you please include this article in the Planning Commission packet?

<https://www.theurbanist.org/2025/05/14/bremerton-ponders-ending-parking-mandates-to-spur-housing/>

Thank you,

Travis

Bremerton Ponders Ending Parking Mandates to Spur Housing

By Travis Merrigan - May 14, 2025



Next Monday, the Bremerton Planning Commission will vote on a reform that could as far as ending parking mandates citywide. The policy has mostly seen smooth sailing, outside of Commissioner Rick Tift's proposal to exempt low-density residential zones that compose 80% of all land available for housing. (Travis Merrigan)

Bremerton seems poised to eliminate parking mandates citywide and adopt a market-based parking policy, as part of the City's Comprehensive Plan update. Majorities on both Bremerton's Planning Commission and City Council seem to be in favor of market-based parking, and public resistance has not materialized. The Planning Commission will vote on Monday, May 19th.

Ending the city's parking mandate would not prevent or prohibit new parking. Instead, new housing and commercial developments would decide on a project-by-project level how much parking is needed, which helps drive down the cost of new housing. Adopting market-based parking (as dozens of US cities have) is a [well-studied policy](#) that results in more housing, increased tax base and reduced dependence on single-occupancy vehicles.

In its 2025 session, Washington State Legislature [passed statewide parking reform](#) that would eventually have forced Bremerton to scale back parking mandates to one stall per two homes in most cases and exempt small commercial spaces and those in mixed-use residential buildings. Cities with populations between 30,000 and 50,000, such as Bremerton, have until 2028 to comply. However, Bremerton appears to be on a proactive course of action, complying early and going beyond the floor the state has set.

Onerous off-street parking requirements

Bremerton's current [off-street parking regulations](#) is vast at 17 pages and more than 5,400 words, with specific parking minimums for 48 different types of businesses – including pool halls, bowling alleys and racquetball clubs. Despite the regulatory ink spilled, Bremerton has nary a pool hall, bowling alley, nor a racquetball club.

Just a fraction of the 17 pages of Bremerton Municipal Code Chapter 20.48, Off-Street Parking Requirements. (City of Bremerton)

Parking mandates for new housing are particularly onerous. A theoretical 80-unit, 5-story apartment block, which might require 16,000 square feet of land for the building itself, but would need nearly 50,000 square feet for the 160 required parking stalls – that's about the size of a football field. If those 160 stalls were built as 'structured' parking inside the building, each stall would add at least \$25,000, or \$4 million in total, to the cost of the building.

Bremerton's current parking regulations make homes more expensive, reduce the total number of new homes built and make urban in-fill housing less profitable. The high cost of developing in the City incentivizes sprawling development on top of former forests in unincorporated Kitsap County. That sprawl increases traffic, vastly expands expenses to maintain roads, sewers and school bus routes, and chokes off growth of Bremerton's property tax base.



Devoting so much land inside a growing city like Bremerton means a continued emphasis on car-oriented development, along with increased pressure to build sprawl in Kitsap's periphery. (Travis Merrigan)

Final debates on parking minimums

All year, parking reform has been advancing as part of Bremerton's 2044 Comp Plan. Elimination of parking minimums seems to have a strong majority on Bremerton's Planning Commission and City Council, and virtually no dissent has yet been heard from the public, or Mayor Greg Wheeler (who rarely weighs in on controversial issues).

In a February meeting, the Planning Commission looked set to **approve recommendations for** "*Staff to remove minimum parking requirements within the Comprehensive Plan... in Land Use policy LU1-Cen(F)... [and] Transportation policy TR5(K)*".

A last minute curveball was thrown in the subsequent April 21st Planning Commission meeting, when Commissioner Richard Tift (who is also a city council candidate) requested a final debate on the topic of excluding 'Low Density Residential' (LDR) zones from parking reforms.



Devoting so much land inside a growing city like Bremerton means a continued emphasis on car-oriented development, along with increased pressure to build sprawl in Kitsap's periphery. (Travis Merrigan)

Final debates on parking minimums

All year, parking reform has been advancing as part of Bremerton's 2044 Comp Plan. Elimination of parking minimums seems to have a strong majority on Bremerton's Planning Commission and City Council, and virtually no dissent has yet been heard from the public, or Mayor Greg Wheeler (who rarely weighs in on controversial issues).

In a February meeting, the Planning Commission looked set to **approve recommendations for** "*Staff to remove minimum parking requirements within the Comprehensive Plan... in Land Use policy LU1-Cen(F)... [and] Transportation policy TR5(K)*".

A last minute curveball was thrown in the subsequent April 21st Planning Commission meeting, when Commissioner Richard Tift (who is also a city council candidate) requested a final debate on the topic of excluding 'Low Density Residential' (LDR) zones from parking reforms.



Low Density Residential accounts for over 80% of all the land in the city of Bremerton that is currently zoned for housing. (City of Bremerton)

Excluding LDR, as Commissioner Tift recommends, would gut parking reform. LDR zones make up 80% of the parts of the City zoned for housing, pushing nearly all affordable housing into just 20% of the City. Excluding LDR would drive up the cost of development over the vast majority of Bremerton, would make many in-fill housing projects impractical and hinder homeowners from adding accessory dwelling units on their properties.

Planning Commissioners — and later City Council — must vote down Commissioner Tift's proposal.

Mission critical: More housing in Bremerton

Bremerton is desperate for more housing. Additionally, the U.S. Navy has recently [announced a multi-billion-dollar plan](#) to expand and modernize Bremerton's shipyards, meaning an immediate influx of 5,000 construction workers and thousands of additional permanent jobs once the expansion is complete.

In 2022, Bremerton's Kitsap County announced a shortage of 9,500 housing units, requiring 1,400 new homes every year. But Kitsap has averaged just over [1,200 new homes](#) per year over the past five years, and over 75% of those were built outside Bremerton city limits, most in sprawling suburban subdivisions. This housing pattern forces Bremerton workers into long commutes, which snarl traffic, increase air pollution, stretch public services like utilities and school bus routes.

Bremerton desperately needs to increase infill housing — homes built within the City, on land already served by streets, utilities and school buses. Bremerton's current parking regulations make new housing much more expensive. And if low-density residential is excluded from parking reform, Bremerton will never realize sufficient infill growth.

Towards market-based parking

Bremerton's move to market-based parking is good government. For decades, Bremerton's development has been governed by heavy-handed, one-size-fits-all regulations that assume well-meaning City officials know better than the people who own the land and seek to build on it.

Homebuilders generally aren't clueless people. They are risking their own money and have every incentive to build homes that people want. They are profit seeking and therefore carefully balance the extra expense of building parking units against the higher price they can charge for parking amenities.

Small, inexpensive apartments near Bremerton's ferries and shipyard may be prized by people who get around on foot or a scooter, or by a single mom on a budget. Larger, more expensive projects would still come with ample off-street parking, often times more than is required.

Market-based parking does not mean no new parking. A [study of Seattle's 2012 parking minimum reduction](#) showed developers still built 60% as many parking stalls as would have previously been required to. Developers didn't eliminate parking, but scaled parking according to local conditions, like availability of transit and walkable amenities.

Bremerton should join the scores of American cities like [Bellingham](#), [Spokane](#), and [Port Townsend](#), and Eugene, Salem, and Beaverton in Oregon that have eliminated parking minimums citywide.

The Bremerton Planning Commission will debate the market-based parking proposal on Monday, May 19th at 5:30pm at City Hall.

[Bremerton Considers Bold Housing Reforms in its Comprehensive Plan Update](#)

[Op-Ed: Bremerton's Multimodal Future at Crossroads with 6th Street Project](#)

Article Author

Travis Merrigan

Travis moved to Washington in 2004, bouncing between Seattle and Spokane, before finding bliss on the Kitsap Peninsula. A bicycle has been his primary mode of transportation since 2002. He has lived in Chicago, San Francisco, and San Juan, Puerto Rico. He holds no antipathy in his heart for humans who disagree with him nor cars. He just wants to make his adopted city of Bremerton safe for his daughters to ride their bikes.

